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AGENDA

PLANNING COMMISSION

CITY HALL COUNCIL CHAMBERS
520 J Street
Los Banos, California

January 25, 2012

If you require special assistance to attend or participate in this meeting, please call the Planning Secretary @ (209) 827-7000 extension 118 at least 48 hours prior to the meeting.

The City of Los Banos complies with the Americans with Disabilities Act (ADA) of 1990.

Si requiere asistencia especial para atender o participar en esta junta por favor llame a la oficina de la Secretaria del Departamento de Planificación al (209) 827-7000 extensión 118 a lo menos de 48 horas previas de la junta.

La Ciudad de Los Banos cumple con la Acta de Americanos con Deshabilidad (ADA) de 1990.

Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda will be made available for public inspection at the meeting and in the Planning Department's office located at City Hall, 520 J Street, Los Banos, California during normal business hours. In addition, such writings and documents may be posted on the City's website at www.losbanos.org.

Cualquier escritura o los documentos proporcionaron a una mayoría del Departamento de Planificación con respecto a cualquier artículo en este orden del día será hecho disponible para la inspección pública en la reunión y en la oficina del Secretaria del Departamento de Planificación del City Hall, 520 J Street, Los Banos, California durante horas de oficina normales. Además, tales escrituras y los documentos pueden ser anunciados en el website de la Ciudad en www.losbanos.org.

- 1 CALL TO ORDER **7:00 PM**
- 2 PLEDGE OF ALLEGIANCE
- 3 ROLL CALL: (Planning Commission Members)
Faktorovich __, Lewis __, Mello __, Toscano __
- 4 APPROVAL OF AGENDA.

Recommendation: Approve the agenda as submitted.

5 ADMINISTRATION OF OATH OF OFFICE TO NEW PLANNING COMMISSIONERS

- Vern Hixson
- Chandra Lee
- Tracey Rosin

Recommendation That the Assistant City Clerk administer the Oath of Office.

6 ROLL CALL. (Planning Commission Members)

Faktorovich __, Hixson __, Lee __, Lewis __, Mello __, Rosin __, Toscano __

7 REORGANIZATION OF PLANNING COMMISSION - SELECTION OF NEW CHAIRPERSON AND VICE CHAIR.

8 CONSIDERATION OF APPROVAL OF THE ACTION MINUTES FOR THE REGULAR MEETING OF DECEMBER 14, 2011

Recommendation: Approve the minutes as submitted.

9 PUBLIC FORUM. Members of the public may address the Commission on any item of public interest that is within the jurisdiction of the Commission, including agenda and non-agenda items. No action will be taken on non-agenda items. Speakers are limited to a five (5) minute presentation

10 PUBLIC HEARINGS If you challenge the proposed action as described herein in court, you may be limited to raising only those issues you or someone else raised at the public hearing described herein or in written correspondence delivered to the City at, or prior to, the public hearing

A. Public Hearing –

- 1) Planning Commission Resolution No. 2012-01 – Recommending approval of the Los Banos Rail Trail Corridor Regulating Code

Recommendation Receive staff report, open the public hearing, receive public comment, close the public hearing and adopt Planning Commission Resolution No. 2012-01 as submitted.

11 PLANNING DEPARTMENT REPORT

12 COMMISSIONERS REPORTS

- A. Faktorovich
- B. Hixson
- C. Lee
- D. Lewis

- E Mello
- F Rosin
- G Toscano

15. ADJOURNMENT

APPEAL RIGHTS AND FILING PROCEDURES

Any person dissatisfied with an act or determination of the Planning Commission may appeal such act or determination to the Planning Commission by filing written notice with the Planning Commission Secretary not later than five (5) business days (excluding holidays) after the day on which the act or determination was made. An appeal must state the act or determination which is being appealed, the identity of the applicant and his/her interest in the matter, and set forth in concise statement(s) the reasons which render the Commission's decision unjustified or inappropriate (Los Banos Municipal Code Section 9-3.2226)

Concerning an action taken by the Planning Commission related to Chapter 2 Articles 1 through 17 of the Los Banos Municipal Code "Subdivisions", if a subdivider or other affected property owner is dissatisfied with any action of the Commission with respect to a tentative map or the nature and extent of improvements recommended or required he/she may within fifteen (15) days after such action appeal to the Planning Commission Secretary for a public hearing on the matter. An appeal must state the action being appealed, identify the agenda item by agency number or project title, and set forth in concise statement(s) the reasons for the appeal. (Los Banos Municipal Code Sections 9-2.807)

Appeals must be in writing and include the appellant's name and address and original signature. A filing fee of \$150.00 must accompany the notice of appeal.

I hereby certify under penalty of perjury under the laws of the State of California that the foregoing agenda was posted on the City Hall bulletin board not less than 72 hours prior to the meeting.


Crystal O'Bannon, Interim Planning Commission Secretary

Dated this 20th day of January, 2012

**CITY OF LOS BANOS
PLANNING COMMISSION MEETING MINUTES
DECEMBER 14, 2011**

ACTION MINUTES – These minutes are prepared to depict action taken for agenda items presented to the Planning Commission. For greater detail of this meeting refer to the electronic media (CD and/or audio) kept as a permanent record.

CALL TO ORDER: Chairman Mello called the Planning Commission Meeting to order at the hour of 7:00 p.m.

PLEDGE OF ALLEGIANCE: Planning Commissioner Lee led the Pledge of Allegiance.

ROLL CALL – MEMBERS OF THE PLANNING COMMISSION PRESENT: Arkady Faktorovich, Vern Hixson, Chandra Lee, Deborah Lewis, Tom Mello, Tracey Rosin, Susan Toscano.

STAFF MEMBERS PRESENT: Planning Director Fitzgerald, City Attorney Vaughn, and Interim Planning Commission Secretary Crystal O'Bannon.

CONSIDERATION OF APPROVAL OF AGENDA: Motion by Lee, seconded by Faktorovich to approve the agenda as submitted. The motion carried by the affirmative vote of all Commission Members present.

CONSIDERATION OF APPROVAL OF ACTION MINUTES FOR THE REGULAR MEETING OF NOVEMBER 9, 2011: Motion by Toscano, seconded by Hixson to approve the minutes as submitted. The motion carried by the affirmative vote of all Commission Members present.

PUBLIC FORUM. MEMBERS OF THE PUBLIC MAY ADDRESS THE COMMISSION ON ANY ITEM OF PUBLIC INTEREST THAT IS WITHIN THE JURISDICTION OF THE COMMISSION; INCLUDES AGENDA AND NON-AGENDA ITEMS. NO ACTION WILL BE TAKEN ON NON-AGENDA ITEMS. SPEAKERS ARE LIMITED TO A FIVE (5) MINUTE PRESENTATION. No one came forward and the public forum was closed.

PUBLIC HEARING – CONSIDER RECOMMENDING APPROVAL OF ANNEXATION AND PREZONE #2011-01, THE DEVELOPMENT AGREEMENT FOR THE PROJECT AND CERTIFICATION OF THE MITIGATED NEGATIVE DECLARATION (SCH #2011101054). ALSO, CONSIDER CONDITIONALLY APPROVING VESTING TENTATIVE PARCEL MAP #2011-02 AND SITE PLAN 2011-03 FOR THE PROPERTY LOCATED AT THE NORTHWEST CORNER OF BADGER FLAT AND PACHECO BOULEVARD; MORE SPECIFICALLY IDENTIFIED AS ASSESSOR'S PARCEL NUMBER: 081-142-013. Planning Director Fitzgerald presented the staff report and PowerPoint presentation.

Commissioner Lewis, noticed from the site plan review that there are multiple lanes that seem to be melting into one lane going across Highway 152; **Ken Anderson, KD Anderson and Associates**, exactly how the lanes will match up is to be determined, Commissioner Lewis, still confused about lanes, **Ken Anderson, KD Anderson and Associates**; the improvements that we have shown there come along with the improvements that WalMart had in place, thinks it may just be an error in illustration, Commissioner Lewis, was on Pacheco at the Project site, isn't sure how far this parcel extends out, the median that is out there now stops in between Taco Bell, isn't sure whether the median will be short of the Project, is concerned that people will try to cross over the Highway; **Ken Anderson, KD Anderson and Associates**, the median goes past the driveway so people turning right only will be able to enter; Commissioner Lewis, just wanted to make sure that nobody can make a left turn from Pacheco going east into the Project; **Ken Anderson, KD Anderson and Associates**, yes that is right; Commissioner Lewis, landscaping and aesthetic concerns, would like to put a condition in place that the trash receptacle doors must always remain closed, concerned about the lack of landscaping between McDonald's and Ronnie's Landscaping, would like to see more vegetation to separate the two properties, Commissioner Hixson; asked if there would be a play place in the McDonald's, **Ken Anderson, KD Anderson and Associates**, no, Commissioner Hixson; confused about the employment numbers, **Jim Abate, McDonald's Franchise**, McDonald's will have approximately 50 employees, at least 10 will be full time employees, expect higher employment for new location, Commissioner Hixson, how can the AM/PM can sell hamburgers at this location and not by Jack in the Box, **Jim Abate, McDonald's Corporation**, is not sure of specifics of each site, looked at several different sites and found that they would not be able to sell their products at other sites, Commissioner Rosin, what will the plans be for the maintenance and upkeep for the remaining portion of the parcel; Planning Director Fitzgerald, misspoke regarding the DA, the remaining portion is subject to the DA, doesn't believe there are any maintenance provisions but maybe Bill can better answer, City Attorney Vaughn, part of the property will be used as a storm drain basin, there will be maintenance requirements in terms of the storm drain basin, Commissioner Toscano, who does weed abatement; City Attorney Vaughn; once it's within the city limits it will be subject to the city laws and ordinances regarding maintenance of vacant property; Commissioner Faktorovich, concerns about the long distance between the gasoline pumps and Badger Flat Road, **Ken Anderson, KD Anderson and Associates**; the McDonald's rep said if they could get another driveway they would be happy but City Staff didn't seem to want another driveway, Commissioner Faktorovich, we have similar situations in Los Banos that cause confusion, Project will require a grease interceptor, is there a connection to the City system; **Randy O'Dell, O'Dell Engineering**, the city has ordinances which require what you talked about, the project proposes to extend the utilities for this project; Commissioner Mello, on the north side off of Badger Flat may want to consider a retaining wall between Ronnie's Landscaping; Planning Director Fitzgerald, it isn't typical to require a wall between two commercial properties, maybe landscaping, Commissioner Rosin; would be against a wall because of the graffiti issue, would rather see landscaping

Chairman Mello opened the public hearing; **Scott Patterson, California Gold Development**; thank you for the consideration of the project, company based out of

Sonora, have been in the business for over 35 years, just finishing a similar project in Sonora, keep all of their projects very clean, landscaping is very important to us, introduced employees, wanted to make points on the annexation, Fiscal Impact study was done and the Project will be beneficial to the City, believes this is not speculative since there is a project going there, there is a lot of offsite work that is done making this an expensive project, will be dedicating almost 1/3 of an acre to road widening, had a few questions that he wanted to ask, dollar for dollar credit on traffic impact fee, would like flexibility when working with power lines, very excited about working in the community, is available for any questions; Commissioner Hixson; once the current McDonald's is vacant what will happen, **Noreen Yeigh, McDonald's Area Real Estate Manager**; we do not own that building so it is up to property owner; Commissioner Lewis, would you have any objections to additional landscaping on the north side of the Project; **Scott Patterson, California Gold Development**; is not opposed and believes there is room for additional landscaping, City Attorney Vaughn, regarding the TIF reimbursement answered in condition 5 of DA, **Scott Patterson, California Gold Development**; appreciates the clarification, because offsite expenses are extensive is there a possibility of a reimbursement agreement, City Attorney Vaughn, suggests that he has a discussion with Public Works before the Project goes to City Council for clarification, Commissioner Lewis, asked to add an additional condition that the trash enclosure door remain closed and that landscaping be added to north boundary line; Planning Director Paula, believes that it definitely would not hurt to add such conditions; Commissioner Hixson, used to be employed by AM/PM believes it's a great company; Commissioner Rosin; do we have to address the underground power question; Planning Director Fitzgerald, the general requirement would be to underground, there will be coordination with PG&E, Commissioner Mello, is happy to see something going on the other side of the road, thinks it will benefit college students.

Motion by Lee, seconded by Faktorovich to adopt Planning Commission Resolution No. 2011-19 – Recommending approval of Annexation and Prezone #2011-01, the Development Agreement for the project and Certification of the Mitigated Negative Declaration (SCH #2011101054) for the property located at the northwest corner of Badger Flat and Pacheco Boulevard The motion carried by the affirmative action of all Commission Members present.

Motion by Lee, Seconded by Faktorovich to adopt Planning Commission Resolution No. 2011-20 – Conditionally approving Parcel Map #2011-02 and Site Plan # 2011-03 for the property located at the northwest corner of Badger Flat and Pacheco Boulevard adding additional landscaping to the northern boundary of the Project site The motion carried by the affirmative action of all Commission Members present.

PLANNING DEPARTMENT REPORT: reported that we have been very busy, has spoken with several people who want to bring forward projects, introduced her mom in the audience, there will be no meeting before Christmas, Merry Christmas and Happy New Year

PLANNING COMMISSION MEMBER REPORTS:

ARKADY FAKTOROVICH: Merry Christmas and wonderful and prosperous New Year.

VERN HIXSON: Introduced his son in law in the audience.

CHANDRA LEE: Nothing to report, have a good holiday

DEBORAH LEWIS: Wishes everyone a wonderful Merry Christmas and a safe and happy New Year

TOM MELLO: Merry Christmas and Happy New Year

TRACEY ROSIN: Reported that Stacy has a new baby girl who is beautiful, was reading the newspaper and noticed that Larry Anderson is in default, Merry Christmas, good luck to Paula in surgery, hopes to be reappointed

SUSAN TOSCANO: Merry Christmas everybody

ADJOURNMENT. The meeting was adjourned at the hour of 7 58 p.m.

APPROVED

Tom Mello, Commissioner

ATTEST

Crystal O'Bannon, Interim Planning Commission Secretary



City of
Los Banos
At the Crossroads of California

Agenda Staff Report

TO: Chair and Planning Commissioners
FROM: Paula Fitzgerald, AICP, Community Development Director
DATE: January 25, 2012
SUBJECT: Rail Trail Corridor Regulating Code
TYPE OF REPORT: Public Hearing

PROJECT BACKGROUND:

The Rail Trail Corridor Regulating Code is the document that was created to refine the vision that was established with the Rail Corridor Master Plan that was approved in early 2006

The original Rail Corridor Master Plan process began in 2002 and was established through a series of public meetings. The purpose of the Master Plan was to provide the direction for land use to ensure implementation consistent with community goals. The Master Plan included a General Plan Land Use amendment that changed the designated land uses to achieve mixed land uses for the purpose of continued economic development in the downtown. The mixed use designation provided for commercial opportunities, multi-family residences, community space, recreational amenities and overall mixed use development to increase the attractiveness and utility of downtown and provide interconnected access through the City's core

In 2008, the City hired RBF Consulting's Urban Design Studio to prepare a more specific vision for the Corridor. The process involved a series of visioning and design workshops with the City Council, Planning Commission, Economic Development Advisory Committee and community members. The Rail Trail Corridor Regulating Code committee traveled to California communities and took pictures of buildings they found acceptable and unacceptable for the Rail Corridor, and in October of 2009 the Regulating Code was completed and presented to the Los Banos Planning Commission

for a recommendation. The Planning Commission unanimously recommended approval but there was some discussion on procedures to implement the Code.

Since that point, the Zoning Map and Ordinance were updated and the Rail Corridor (R-C) Zoning Classification was created to allow for and facilitate the envisioned types of development within the Rail Trail Corridor Regulating Code.

Staff has completed a full review of the document and has made some revisions relating to housekeeping issues, the actual administration of the Code so that applications will be required to go to the Planning Commission and the Street section portion so that the Street Sections follow the Improvement Standards and Specifications and General Plan EIR.

ENVIRONMENTAL:

The Rail Trail Corridor Regulating Code has been adequately analyzed in the Rail Corridor Master Plan EIR, The General Plan EIR and the Zoning Ordinance Negative Declaration. All the above mentioned documents are available at the Community Development Department at: 520 J St., Los Banos, CA 93635. Adoption of the Rail Trail Regulating Code will not result in any new impacts. The Rail Trail Corridor Regulating Code is a planning document which does not provide entitlements for any project. All projects within the City will require separate environmental review in accordance with CEQA Guidelines.

RECOMMENDATION:

Staff recommends that the Commission receives the staff report, open the public hearing, take public comment, and recommend approval of the Rail Trail Corridor Regulating Code.

Attachments.

- 1) Resolution 2012-01
- 2) Rail Trail Corridor Regulating Code

RESOLUTION NO. 2012-01

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LOS BANOS RECOMMENDING APPROVAL OF THE LOS BANOS RAIL TRAIL CORRIDOR REGULATING CODE

WHEREAS, IN 1998 The Los Banos City Council adopted the Los Banos Rail Trail Corridor Feasibility Plans, and

WHEREAS, this plan explored the idea of eliminating blight by removing the railroad ties that had been abandoned by the Union Pacific Railroad and renovating the property for use as an economic tool to help revitalize the downtown areas of Los Banos, and

WHEREAS, in 2005 the Los Banos City Council approved the Rail Trail Corridor Land Use and Zoning Map, and

WHEREAS, in 2008 RBF Consulting was hired by the Los Banos Redevelopment Agency to create a Vision Poster and Regulating Code; and

WHEREAS, members of the community, Los Banos City Council, Los Banos Planning Commission, and Los Banos Economic Development Committee gave input for this plan; and

WHEREAS, this Regulating Code follows the Los Banos General Plan; and

WHEREAS, the Regulating Code was recommended for approval by the Los Banos Planning Commission on December 9, 2009; and

WHEREAS, some minor modifications were made to the document so it was brought back to the Planning Commission for a recommendation, and

WHEREAS, the Planning Commission held a public hearing on the matter on January 25, 2012; and

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Los Banos does hereby recommend approval of the Los Banos Rail Corridor Regulating Code.

The foregoing Resolution was introduced at a regular meeting of the Los Banos Planning Commission by Commissioner _____ and seconded by Commissioner

_____ on the 25^h day of January, 2012. The Resolution was adopted by the following vote:

AYES:
NOES:
ABSENT:

APPROVED

Planning Commission Chair

ATTEST

Crystal O'Bannon, Interim Planning Commission Secretary

**LOS BANOS
RAIL TRAIL CORRIDOR
REGULATING CODE**

Draft January 2012

City of Los Banos
520 J Street
Los Banos, CA 93635
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www.losbanos.org

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1.0 Introduction and User's Guide

1.1 Background

The Los Banos Rail Trail Corridor (the Corridor) is an approximately 60-acre area located within the City of Los Banos (see Figure 1-1 Location Map). H Street, G Street, Second Street, and a proposed extension of Santa Rita Avenue generally define the borders of the Rail Trail Corridor (see Figure 1-2: Rail Trail Corridor Boundary)

Most of the Rail Trail Corridor was previously owned by the Union Pacific Railroad (UPRR). The City of Los Banos Redevelopment Agency purchased land from the UPRR in 2005, and subsequently the City prepared a land use plan to redevelop the Corridor with a multipurpose trail, a public plaza, park space, and a mix of residential, commercial, and civic uses. The multipurpose trail (the Rail Trail) and Henry Miller Plaza were constructed in 2005 and the Community Center was constructed in 2009. To date, the other properties along the Rail Trail have not been redeveloped and most of the land is vacant. A few properties are developed with residential apartments and industrial buildings. Figure 1-3 (Oblique Aerial) shows the existing conditions of the Corridor and surrounding areas minus the Community Center. Included is a view of Downtown Los Banos, which is located south of the Rail Trail Corridor. Sixth Street, which is perpendicular to the Rail Trail Corridor, is the "Main Street" of Downtown Los Banos.

In 2007, the City hired RBF Consulting's Urban Design Studio to prepare a more specific vision for the Corridor. The process involved a series of visioning and design workshops with the Planning Commission, City Council, and community members. A vision plan and this Regulating Code were prepared for the Corridor based on the input from the workshop participants. Section 2.0 (Vision) describes the vision for the future of the

Rail Trail Corridor. In 2012, the City Staff made minor revisions to present to the Planning Commission and City Council, keeping the same concepts and ideas.

Figure 1-1: Location Map

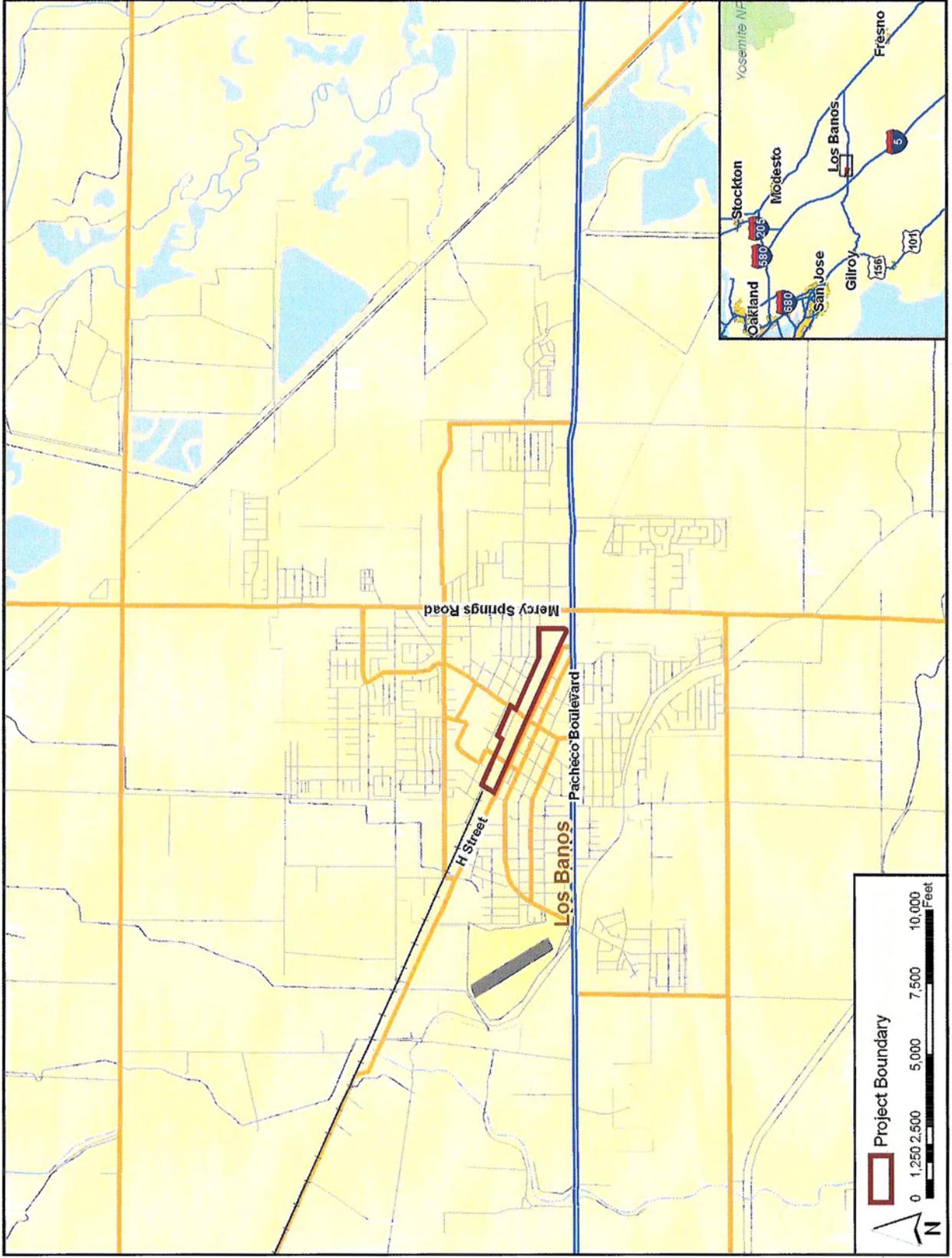
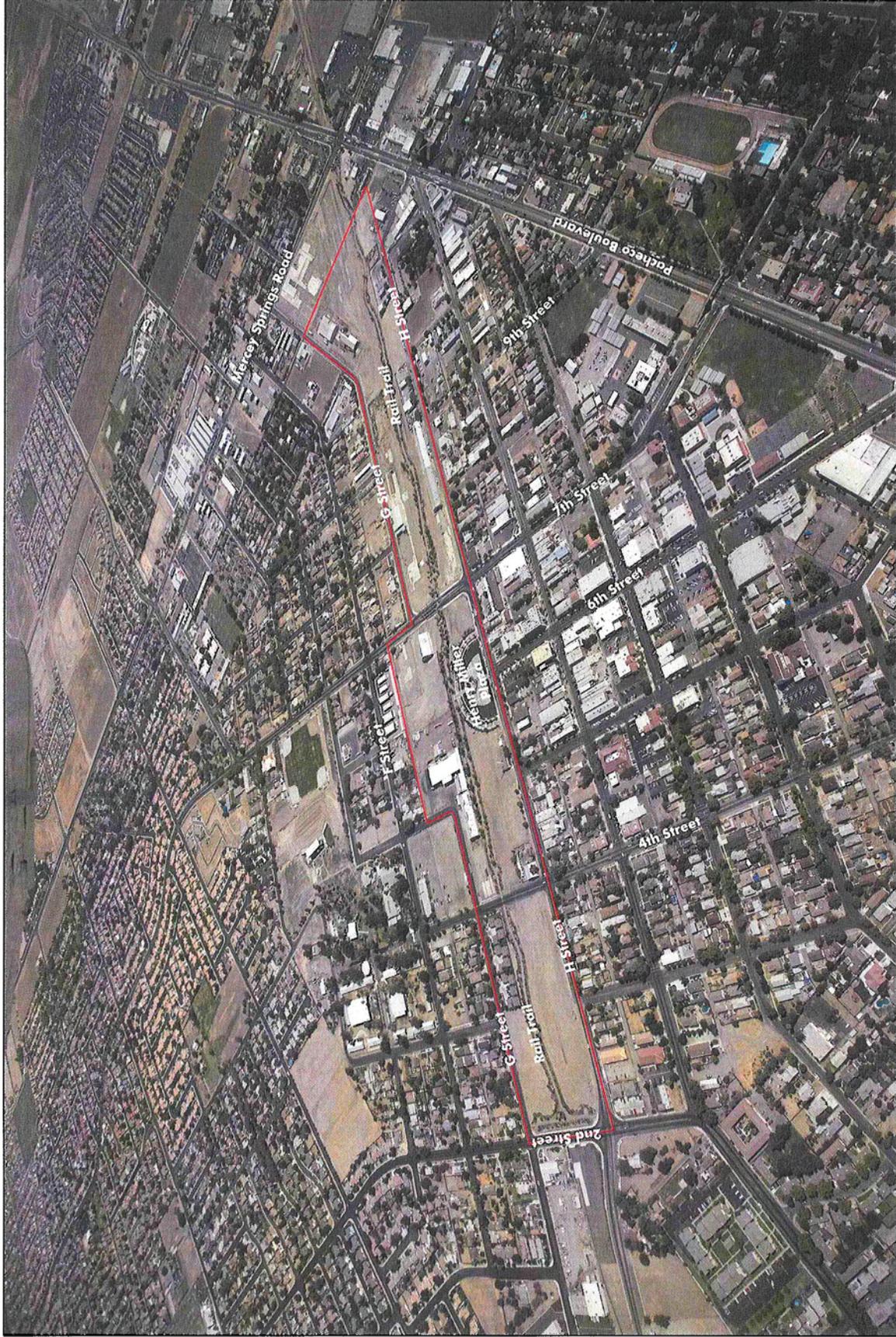


Figure 1-2: Rail Trail Corridor Boundary



Figure 1-3: Oblique Aerial



1.2 Purpose and Intent

To implement the envisioned future of the Rail Trail Corridor, new zoning and development standards are required. The purpose of this Regulating Code is to establish appropriate zoning and development standards that allow and facilitate the envisioned types of development within the Rail Trail Corridor. The zoning and development standards within this Regulating Code are intended to:

- Allow a mix of development types that create a compact, pedestrian-oriented neighborhood that is linked by the existing Rail Trail.
- Allow a mix of compatible residential, commercial, and civic uses that complement the businesses within Downtown Los Banos.
- Provide a variety of housing types to promote diversity
- Ensure that buildings have a positive relationship to surrounding streets, the Rail Trail, Henry Miller Plaza, and future public spaces within the Corridor
- Establish form-based development standards that will facilitate the envisioned character for the area.

1.3 Relationship to the Zoning Ordinance

On October 20, 2010, the City of Los Banos adopted an updated Zoning Ordinance. The Updated Zoning Ordinance includes the Rail Corridor (R-C) Zone. Per the requirements of Section 9-3.502, properties within the R-C Zone are subject to the standards within this Regulating Code. The standards within this Regulating Code become the legal zoning standards for these properties. The Municipal Code shall be used for topics not covered by this Regulating Code. Should any conflicts between this Regulating Code and the Municipal

Code arise, the provisions within the Municipal Code shall apply

1.4 User's Guide

The following steps will help users understand this Regulating Code:

- Step 1: Review Section 2 0 to gain an understanding of the vision for the Rail Trail Corridor
- Step 2: Review Section 3 0 to gain an understanding of the types of buildings and public spaces that are allowed within the Corridor. Review the Regulating Plan (Figure 3-1) to determine where building types and public spaces are allowed.
- Step 3: Review Section 4.0 to gain an understanding of the subdivision standards for blocks along the Corridor.
- Step 4: Review Section 5.0 to gain an understanding of the development standards for each of the allowed building types.
- Step 5: Review Section 6 0 to determine the additional architectural design standards that could apply to the project.
- Step 6: Review Section 7 0 to determine the standards for streetscape improvements and the types of new streets that are required in the Corridor
- Step 7: Review Section 8.0 to gain an understanding of the types of public spaces that are required within the Corridor, and to review design standards for each type of public space.
- Step 8: Review Section 9.0 to gain an understanding of the administration of this Regulating Code.

- Step 9: Review Section 10.0 to gain an understanding of terms and acronyms used throughout this Regulating Code.
- Step 10 Contact the City of Los Banos to discuss questions, potential projects, or opportunities for public/private partnerships:

City of Los Banos

520 J Street

Los Banos, CA 93635

(209) 827-7000

www.losbanos.org

2.0 Vision

The Rail Trail Corridor is envisioned to be a unified, pedestrian-oriented district with a mix of compatible commercial and residential uses within attractive buildings that front inviting public gathering spaces and tree-lined streets.

Properties are envisioned to redevelop with multi-story buildings that have a strong relationship and orientation to streets, the Rail Trail, and existing and new public spaces. Existing streets will be improved with on-street parking, expanded sidewalks, street trees, pedestrian-scaled streetlights, and other sidewalk amenities. New streets, alleys, and pedestrian connections will provide greater access between the Rail Trail, Downtown Los Banos, and surrounding neighborhoods. New public spaces will include a variety of features, such as pedestrian paths, playgrounds, fountains, seating areas, and outdoor dining areas.

The Rail Trail Corridor will serve as a transition between Downtown Los Banos and neighborhoods to the north. Development along the southern portion of the Rail Trail is generally envisioned to have a mix of commercial, residential, and live-work units. Development along the north is generally envisioned to have more residential and civic uses.

The Corridor will include a bus depot at the intersection of H Street and Fourth Street. The bus depot will improve transit opportunities within both sides of the Downtown area.

A family entertainment center is envisioned on either the Rail Trail between Second Street and the Third Street Rail Trail Connection. This center is envisioned to be housed in two buildings with a strong connection between each.

New development and redevelopment is encouraged to provide sustainable design and smart growth principles that encourage compact development and a healthy community.

The Vision Plan and the sketches on the following pages illustrate the envisioned future of the Rail Trail Corridor. The Vision Plan and sketches are for illustrative purposes only, and they are not to be used as a form of regulation.

Figure 2-1: Vision Plan (Front of Poster)

Los Banos Rail Trail Corridor Draft Vision Plan

The Rail Trail Corridor is envisioned to be a unified, pedestrian-oriented district with a mix of compatible commercial and residential uses and inviting public gathering spaces (including a bus depot and plaza), all of which are linked by the existing Rail Trail. Properties are envisioned to be developed with buildings that have a strong relationship and orientation to streets, the Rail Trail, Henry Miller Plaza, and new public spaces.

The Corridor will link Downtown Los Banos with nearby neighborhoods. New pedestrian connections will be created to provide access from H Street and G Street to the Rail Trail. These Rail Trail Connections will include a variety of features, including plaza and park space, playgrounds, fountains, and landscaping. Plazas will be lined with shops, restaurants, and cafes. A family entertainment center may be developed near the West Entry.

1 West entry with promenade adjacent to family entertainment center

2 Henry Miller Plaza with mixed-use buildings and space for special events

3 Eye-level view of mixed-use buildings along H Street

4 Clock tower and corner plazas at the H Street and 7th Street intersection

5 Townhouses surrounding a Rail Trail Connection with playground

6 View of the median park from adjacent condominium unit

This vision plan provides a look into the future of what the Rail Trail Corridor can become. It offers the City, property and business owners, the real estate community, and residents an illustration of one of many scenarios for the future of the Corridor. This vision plan should not be viewed as an absolute plan or form of regulation due to the many unforeseen conditions, opportunities, and constraints that may arise in the future.

October 2009
Prepared for the
City of Los Banos by:

Figure 2-1: Vision Plan (Back of Poster)

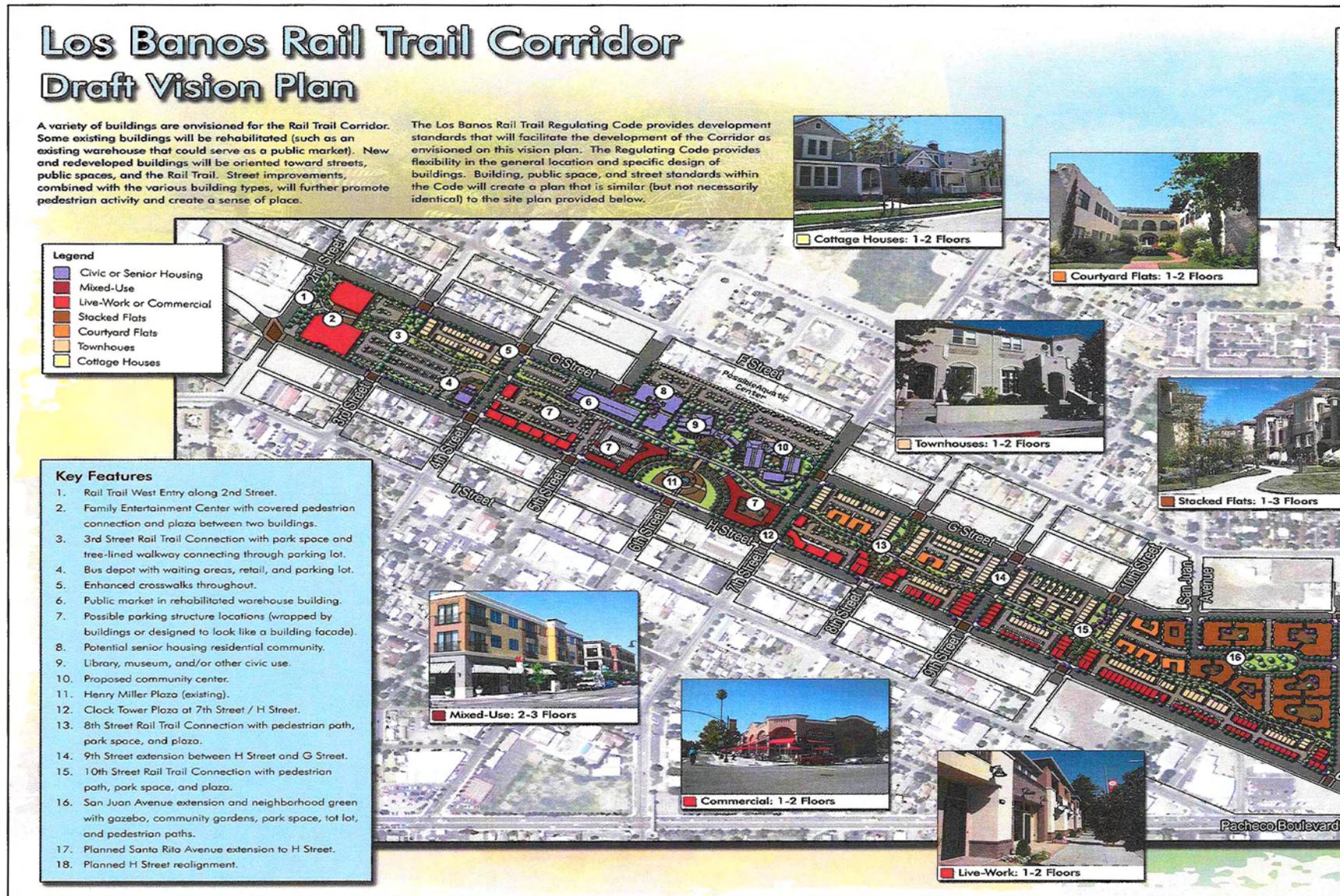


Figure 2-2: Envisioned Corridor



Figure 2-3: West Entry and Family Entertainment Center



Figure 2-4: Townhouses Surrounding a Rail Trail Connection with Playground



Figure 2-5: Henry Miller Plaza



Figure 2-6: H Street

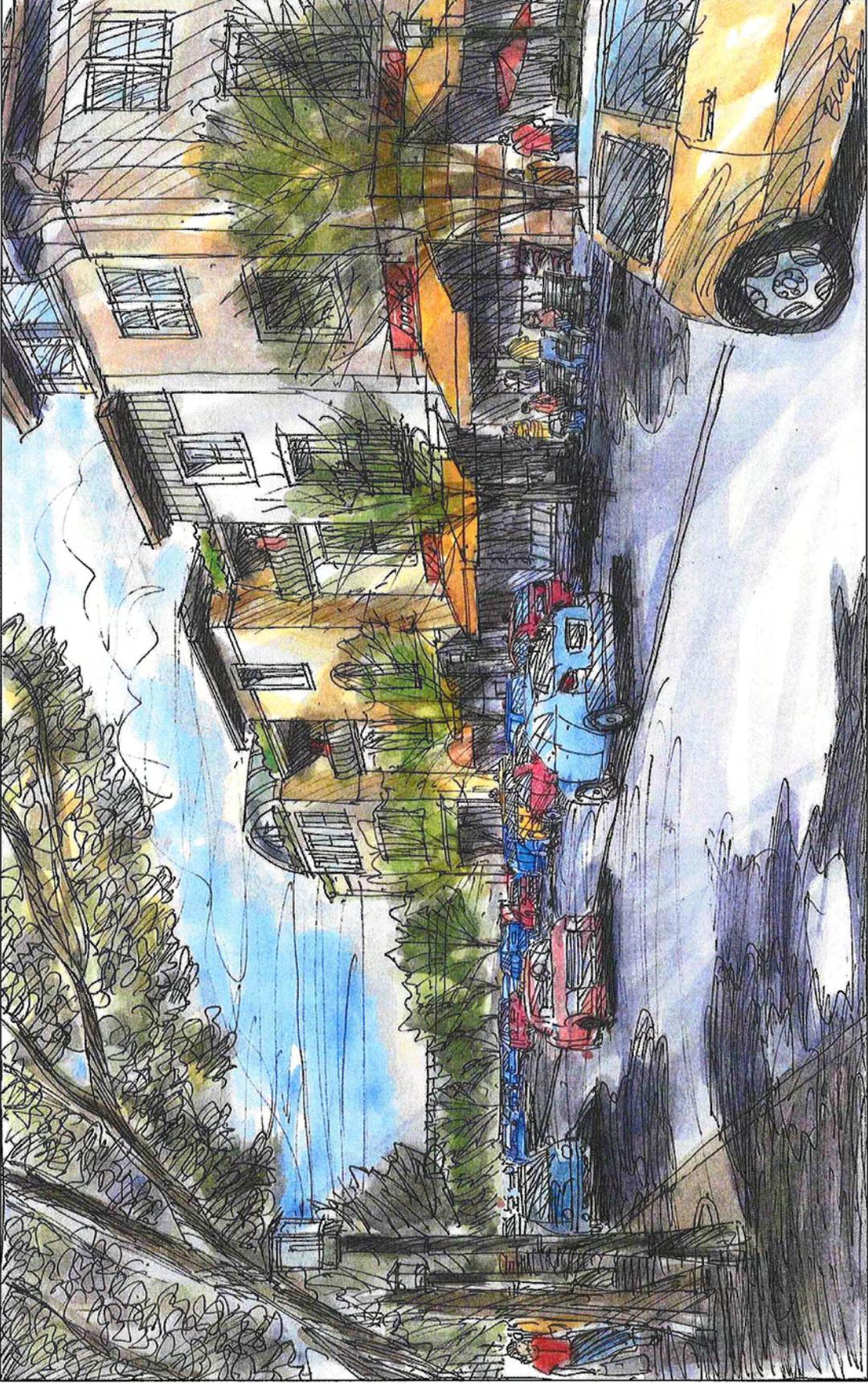


Figure 2-7: Corner Plazas at Seventh Street and H Street

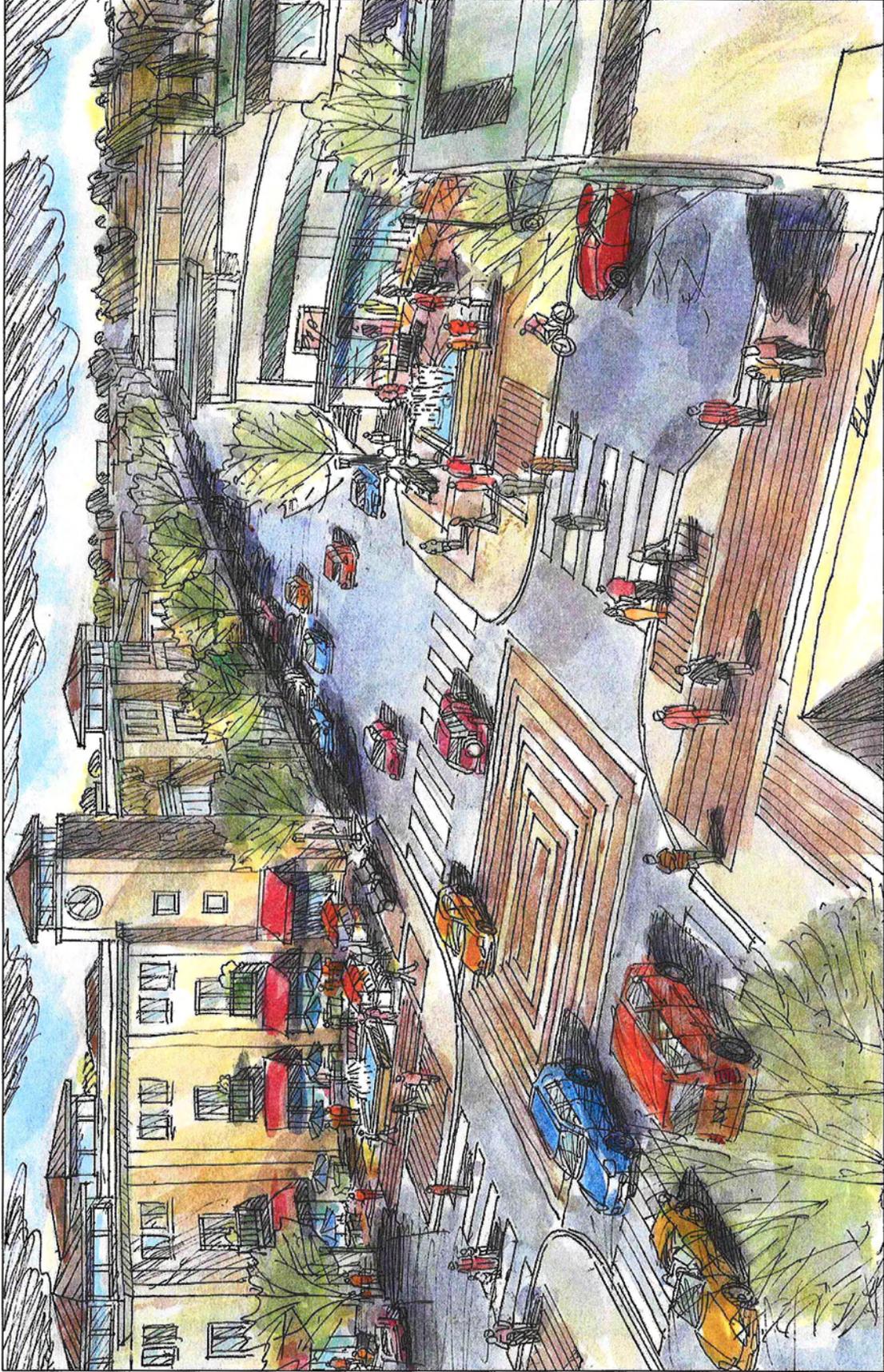
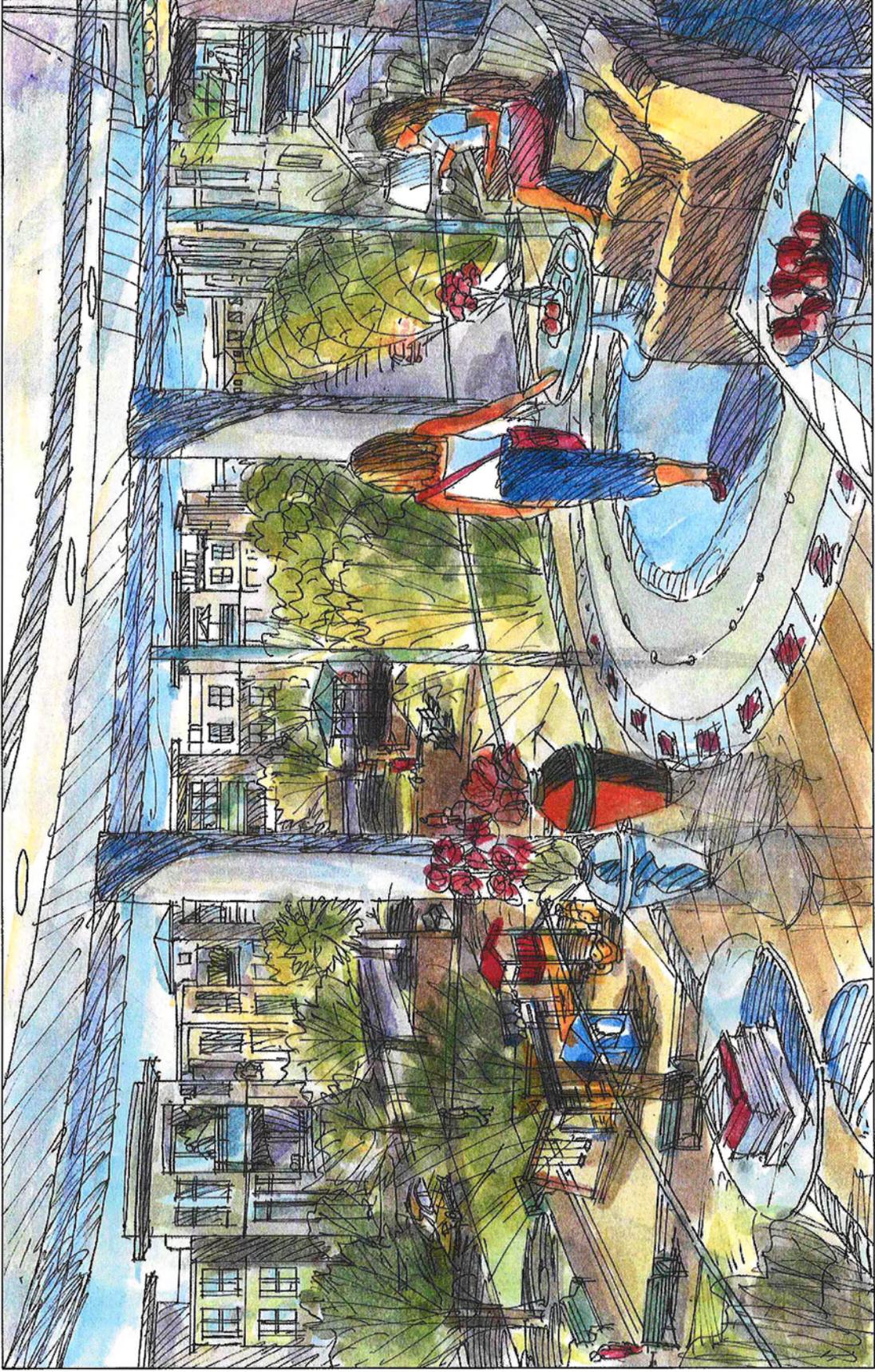


Figure 2-8: Apartments near Median Park



3.0 Building Types and Regulating Plan

3.1 Building Types and Public Spaces

The Rail Trail Corridor is envisioned to be developed with a variety of building types and public spaces. These building types and public spaces are briefly described below.

Building Types

- **Mixed-Use Buildings:** Two to three story buildings designed for a mix of commercial and residential uses. The ground floor generally contains storefronts for retail stores, restaurants, and cafes. Upper floors generally contain apartments, condominiums, and/or office space
- **Commercial Buildings:** One or two story buildings designed for a mix of commercial uses, including retail, restaurants, cafes, and office uses.
- **Live-Work Buildings:** One to two story buildings that contain units in which the owner can both live and operate certain types of office and service businesses.
- **Stacked Flats:** Two to three story buildings designed with stacked residential units (condominiums or apartments). Parking is provided in a podium parking level or a surface parking lot that is surrounded by the residential units
- **Courtyard Flats:** Two story buildings designed for stacked residential units (condominiums or apartments). The residential units create a "U" shaped building that surrounds a common courtyard.
- **In-Line Townhouses:** Two story residential units that are placed side-by-side to form a row of houses that face a street, the Rail Trail, or Rail Trail Connection. The garage is accessed from a rear alley
- **Cluster Townhouses:** Two story residential units that are placed side-by-side to form two rows of houses that face each other and are separated by a common green. The garage is accessed from a rear alley
- **Rear Yard Townhouses:** Two story residential units that are placed side-by-side to form a row of houses that face a street. Each unit contains a yard that is located between the unit and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.
- **Cottage Houses:** One to two story houses that have yards and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.
- **Family Entertainment Center:** One or two buildings designed with entertainment uses, such as an arcade, restaurants, party rooms, bowling alleys, and indoor miniature golf
- **Public/Civic Buildings:** Buildings designed for public or civic purposes, such as a community center, bus depot, library, museum, senior housing, public market, or aquatic center.

Public Spaces

- **Rail Trail:** An existing multi-purpose trail that is located on the abandoned Union Pacific Railroad corridor and extends from Second Street to the intersection of Mercy Springs Road and Pacheco Boulevard.
- **Rail Trail Connection:** A planned linear plaza and/or park that connects an existing or planned street to the Rail Trail.
- **Henry Miller Plaza:** An existing semi-circular public plaza that is located along H Street at the terminus of Sixth Street
- **Corner Plaza:** One of two planned plazas located at the northwest and northeast corners of Seventh Street and H Street
- **Median Park:** A neighborhood park located within the median of a planned street that would extend from the San Juan Avenue Extension to the Santa Rita Avenue Extension.
- **West Entry:** A planned public space at the western entrance to the Rail Trail on Second Street.

3.2 Regulating Plan

A regulating plan is a map or plan that designates where certain building types and public spaces are allowed and/or required. The Regulating Plan for the Rail Trail Corridor is provided in Figure 3-1. The Regulating Plan divides the existing blocks within the corridor into a variety of development sites that are defined by new alleys, public spaces, and streets. Each development site is designated as a specific zone. Within each zone, there are certain types of building types that are allowed.

Section 4.0 (Subdivision Standards) contains standards that guide the subdivision of blocks into the development sites shown on Figure 3-1. These standards will create a system of development sites, alley networks, and blocks that are similar, but not necessarily identical to those illustrated in Figure 3-1. The locations and layouts of streets, alleys, blocks, development sites, and public spaces as illustrated in the Regulating Plan are illustrative in nature and subject to change in refinement through the subdivision process. Minor adjustments to the development site boundaries and zone boundaries may result, as long as the subdivision complies with the applicable standards in Section 4.0.

Section 5.0 (Site and Building Standards) contain the development standards for each building type identified in the Regulating Plan. For each building type, there are standards for pedestrian connections, site design, height and mass, window fenestration and frontages, projections, building uses, and parking requirements.

Figure 3-1: Regulating Plan



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4.0 Subdivision Standards

To implement the vision for the Rail Trail Corridor, existing blocks will need to be subdivided into multiple development sites. This section establishes the development standards for the subdivision of blocks into development sites

The following terms are used in this section:

- **Block:** An area that is completely surrounded by a combination of streets, the existing Rail Trail, a planned Rail Trail Connection, or the planned Fifth Street Public Parking Lot.
- **Development Site:** A portion of a block where an individual building type and related site features are allowed.
- **Alley:** A privately owned and maintained roadway that provides access from streets to the rear of each development sites within a block.

Section 3.0 (Building Types and Regulating Plan) provides definitions for additional terms that are used in this section.

4.1 Block Types

This section provides standards for subdividing land within the Rail Trail Corridor based on block type. Figure 4-1 (Block Types) shows the location of each block type.

Figure 4-1: Block Types



4.2 Subdivision Standards

With the exception of Block Type G, all blocks undergoing redevelopment shall be subdivided to create development sites, public spaces, and alley networks. Subdivision standards for each block type are provided on the following pages.

Standards are not provided for Block Type G. This block is intended for public/civic uses and does not require further subdivision.

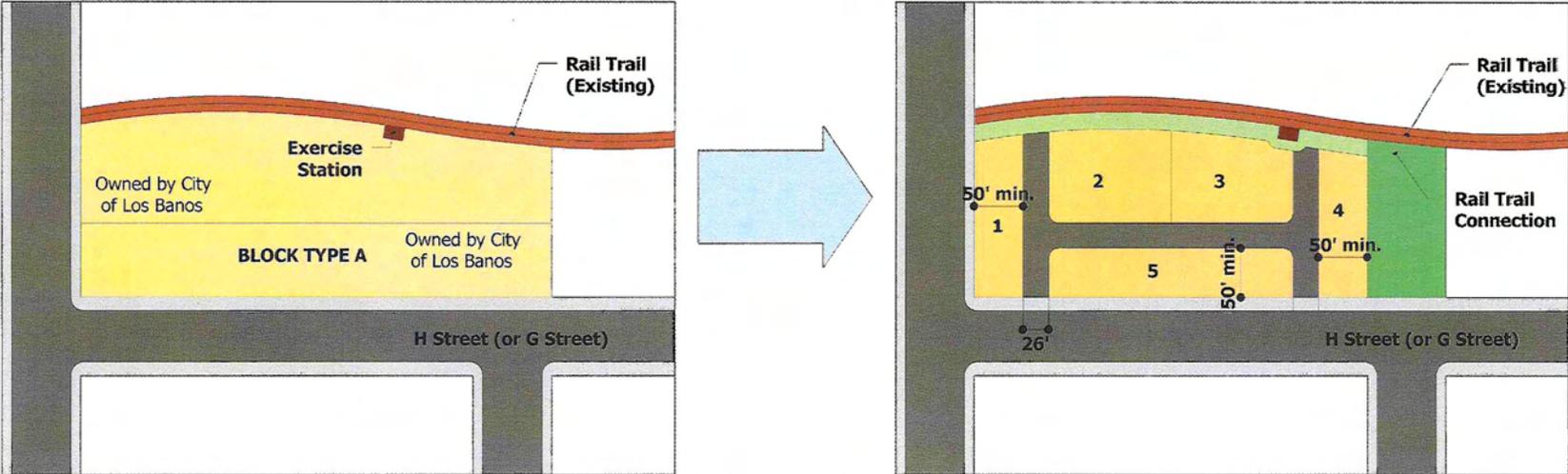
Block Type A

The blocks identified as Block Type A on Figure 4-1 shall be subdivided into development sites based on the following standards:

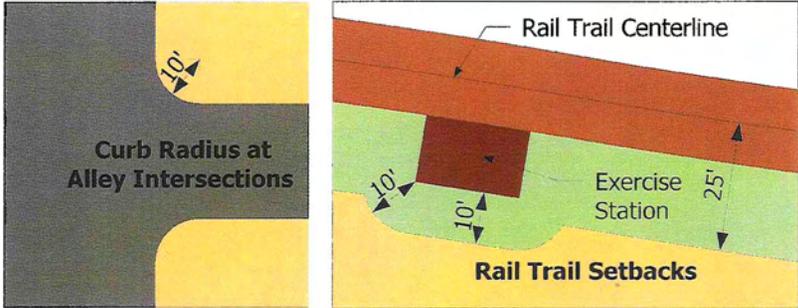
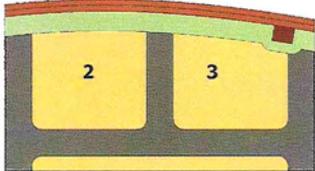
- A. The block shall be subdivided into five development sites and a private alley network. The specific location, design, and size of the development sites and the alley network may vary from block-to-block provided that the standards below are achieved.
- B. Development sites are required along the edges of all sides of the block (see Figure 4-2).
- C. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- D. The alley network shall have an “H” shaped configuration that provides access to the back of all development sites (see Figure 4-2). The alley network shall connect to H Street (or G Street) at two locations. Alleys shall not connect to numbered streets (Second Street, Fourth Street, Seventh Street, and Ninth Street).
- E. All alleys shall be 26-feet wide.
- F. At all alley intersections, a 10-foot curb radius shall be provided.
- G. All development sites shall have a minimum depth of 50 feet. Larger depths are required to accommodate certain building types, such as Cluster Townhouses (70 feet), Courtyard Flats (70 feet), Rear Yard Townhouses (80 feet), and Cottage Houses (80 feet).
- H. Rail Trail Connections shall be established at the following locations:
- Along the former right-of-way of Third Street between H Street and the existing Rail Trail.
 - Along the former right-of-way of Third Street between G Street and the existing Rail Trail.
 - Along the former right-of-way of Eighth Street between G Street and the existing Rail Trail.
 - Along the former-right-of-way of Tenth Street between G Street and the existing Rail Trail.

The Rail Trail Connections noted above shall be 80 feet wide. Section 8.1 provides the standards for the design of Rail Trail Connections.

Figure 4-2: Example Subdivision of Block Type A



An alley between Development Sites 2 and 3 is required if Development Site 2 or 3 (or both 2 and 3) contain Clustered Townhouses

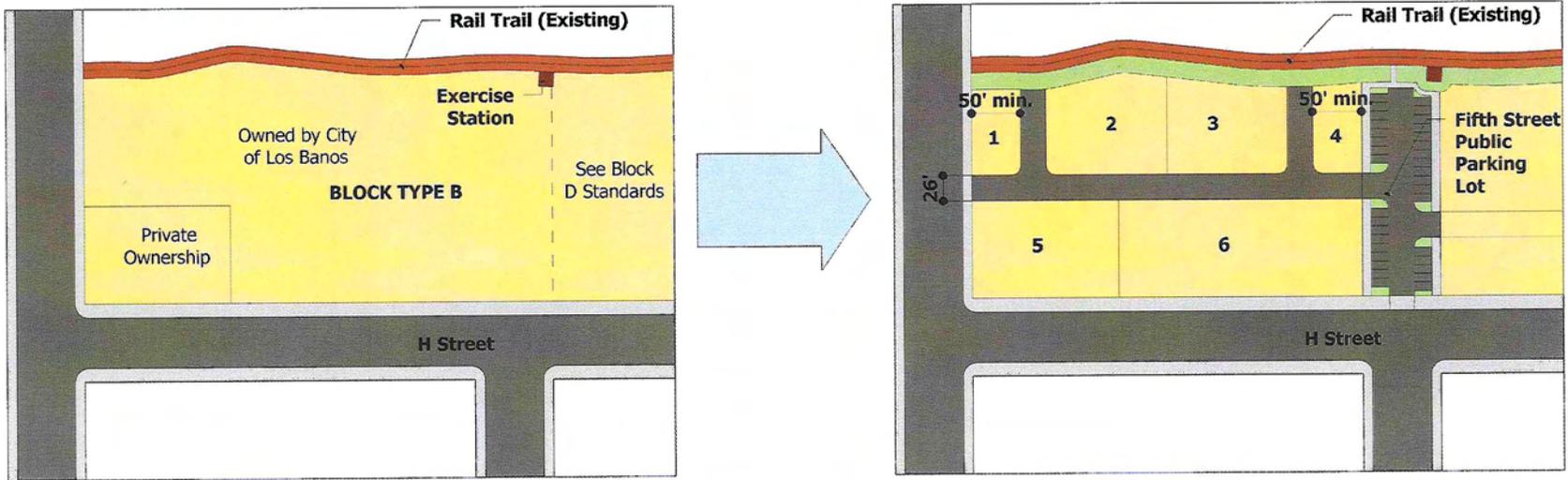


Block Type B

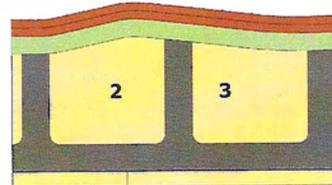
The block identified as Block Type B on Figure 4-1 shall be subdivided into development sites based on the following standards:

- A. The block shall be subdivided into six development sites and a private alley network. Development sites are required along the edges of all sides of the block (see Figure 4-3).
- B. The alley network shall provide access to the back of all development sites (see Figure 4-3). The specific location and design of the alley network may vary provided that the standards below are achieved.
- C. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- D. If additional parking is desired for surrounding uses, Development sites 2, 3, and 4 and the alley between development sites 3 and 4 may be developed as a public parking lot (see Figure 4-3).
- E. The alley network shall connect to Fourth Street and the proposed Fifth Street Public Parking Lot. Alleys shall not connect to H Street.
- F. All alleys shall be 26 feet wide.
- G. At all alley intersections, a 10-foot curb radius shall be provided.
- H. All development sites shall have a minimum depth of 50 feet. Larger depths are required to accommodate certain building types, such as Cluster Townhouses (70 feet) and Courtyard Flats (70 feet).
- I. The proposed Fifth Street Public Parking Lot shall be established within the former right-of-way of Fifth Street between H Street and the existing Rail Trail.

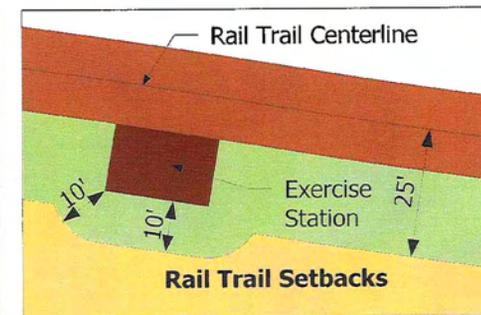
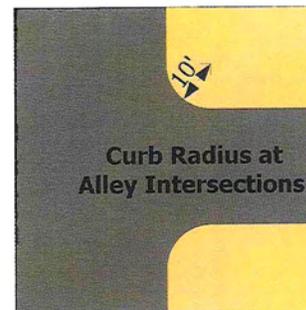
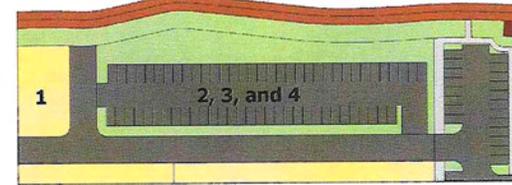
Figure 4-3: Example Subdivision of Block Type B



An alley between Development Sites 2 and 3 is required if Development Site 2 or 3 (or both 2 and 3) contain Clustered Townhouses



Development sites 2, 3, and 4 and the alley between development sites 3 and 4 may be developed as a public parking lot



Block Type C

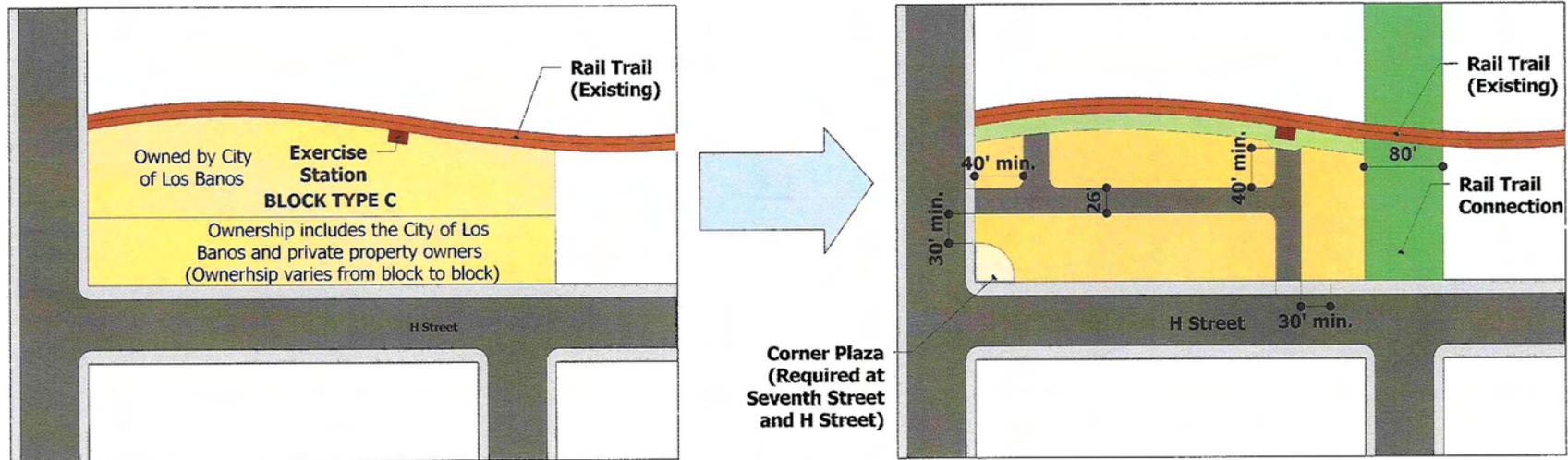
The blocks identified as Block Type C on Figure 4-1 are relatively small blocks that are partially owned by the City of Los Banos and private property owners. The private and public lots have relatively small lot depths, which limits the amount of development that can occur on each lot. Therefore, the City encourages the merger of lots through a public-private partnership or through lot consolidation to maximize the development potential of each block. As an incentive to forming this partnership, the City may consider a land swap that would give the private property owners the adjacent City-owned parcels in exchange for the land required for Rail Trail Connections and Corner Plazas.

The blocks identified as Block Type C on Figure 4-1 shall be subdivided into development sites based on the following standards:

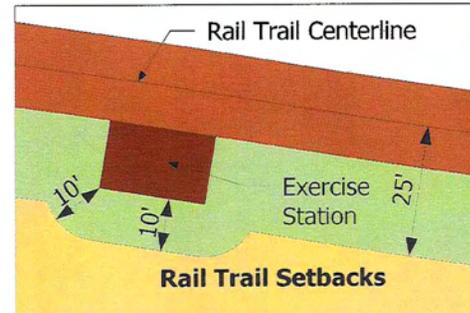
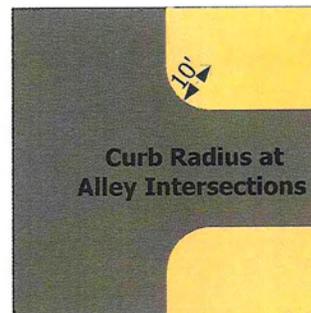
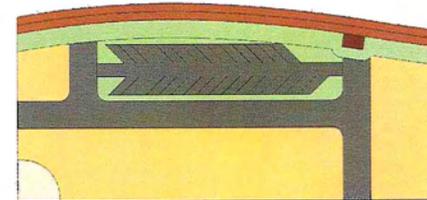
- A. Each block shall be subdivided into multiple development sites and a private alley network. At least one development site is required along all four edges of the block. The alley network shall provide access to the back of all development sites (see Figure 4-4). The specific location and design of the alley network may vary from block-to-block provided that the standards below are achieved.
- B. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- C. The alley network may connect to H Street, numbered streets (Seventh Street and Ninth Street), and the Santa Rita Avenue extension
- D. All alleys shall be 26 feet wide.
- E. At all alley intersections, a 10-foot curb radius shall be provided.
- F. All development sites shall have a minimum depth of 40 feet. The Planning Commission may allow development sites with depths of 35 feet in areas where space is limited due to the Rail Trail's proximity to H Street. In order to meet Standard G, larger depths are required for Development Sites that are adjacent to Rail Trail Connections and Corner Plazas.
- G. Development sites that are adjacent to the Rail Trail (but that are not also adjacent to a street) may be developed as parking for other development sites within the block.
- H. Alleys shall be located at least 30 feet from the edges of all Rail Trail Connections and Corner Plazas
- I. Rail Trail Connections shall be established at the following locations:
 - Along the former right-of-way of Eighth Street between H Street and the existing Rail Trail.
 - Along the former-right-of-way of Tenth Street between H Street and the existing Rail Trail.

The Rail Trail Connections noted above shall be at least 80 feet wide. Section 8.1 provides the standards for the design of Rail Trail Connections.
- J. A Corner Plaza shall be established at the northeast corner of Seventh Street and H Street. The plaza shall be defined by a curved arc that has a radius of at least 35 feet. The plaza shall occupy at least 1,330 square feet. Section 8.2 provides the standards for the design of Corner Plazas.

Figure 4-4: Example Subdivision of Block Type C



The development site that is adjacent to the Rail Trail (but that is not also adjacent to a street) may be developed as parking for other development sites on the block

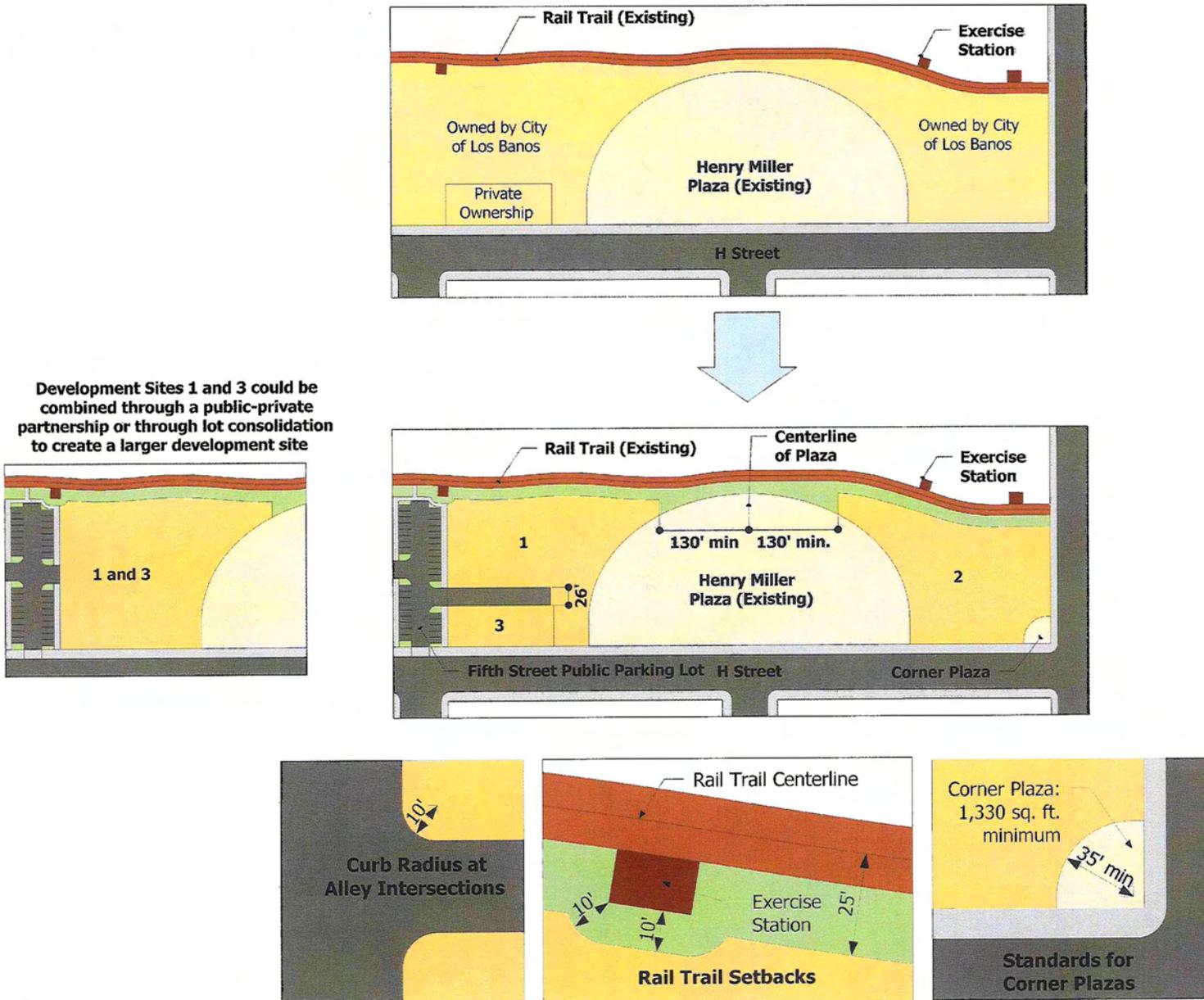


Block Type D

The block identified as Block Type D on Figure 4-1 shall be subdivided into development sites based on the following standards:

- A. The block shall be subdivided into two or three development sites (see Figure 4-5).
- B. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- C. The edges of development sites shall be setback at least 130 feet from the centerline of Henry Miller Plaza
- D. An alley shall be located between Development Sites 1 and 3. The alleys shall be 26 feet wide and 150 feet long. The alley shall provide access to the parking facilities within Development Sites 1 and 3. This standard does not apply if Development Sites 1 and 3 are merged through a public-private partnership or lot consolidation.
- E. A Corner Plaza shall be established at the northwest corner of Seventh Street and H Street. The plaza shall be defined by a curved arc that has a radius of at least 35' feet. The plaza shall occupy at least 1,330 square feet. Section 8 2 provides the standards for the design of Corner Plazas

Figure 4-5: Example Subdivision of Block Type D

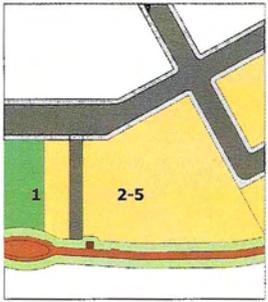
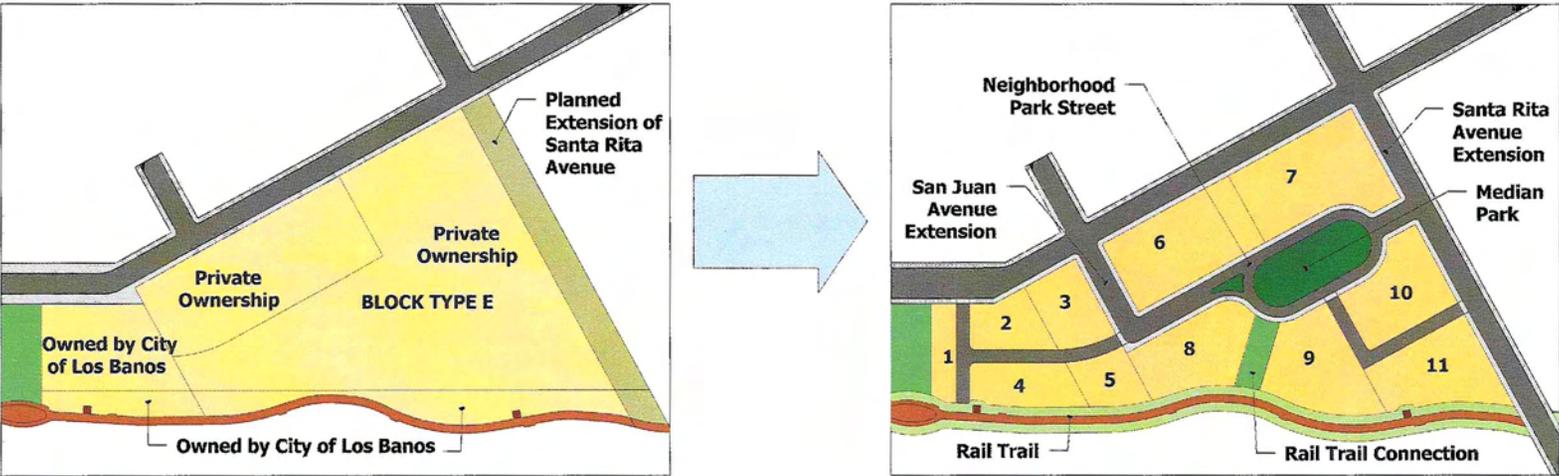


Block Type E

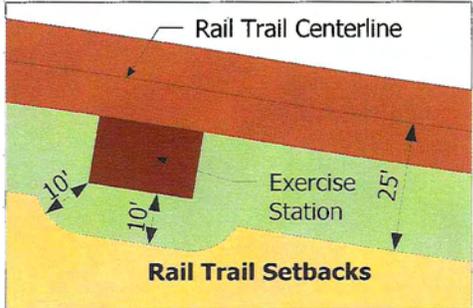
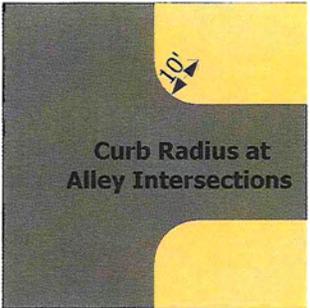
The block identified as Block Type E on Figure 4-1 is currently owned by both the City of Los Banos and private property owners. The City and private property owners are encouraged to form a public-private partnership for the development of the block. As an incentive to forming this partnership, the City may consider a land swap that would give the private property owners the City-owned parcels in exchange for the land required for the new streets, the Median Park, and the Rail Trail Connection.

- A. The block shall be subdivided into multiple development sites that are defined by existing and new streets, the existing Rail Trail, and a private alley network. The exact locations and layouts of the streets, alleys, development sites, the Median Park, and the Rail Trail Connection are fairly flexible and subject to change and refinement through the subdivision process. The design of Block E as illustrated on the Regulating Plan (Figure 3-1) and the drawings on the following pages are for illustrative purposes only. The actual design of Block E may vary from the illustrations, provided that the following standards are achieved.
- B. San Juan Avenue shall be extended into the block and shall connect with a new Neighborhood Park Street that connects with the planned Santa Rita Avenue Extension.
- C. The Neighborhood Park Street shall contain a Median Park that is at least 100' wide and occupies at least 22,800 square feet. Section 8.3 provides the standards for the design of the Median Park.
- D. A Rail Trail Connection shall link the Median Park to the Rail Trail. The Rail Trail Connection shall be at least 26 feet wide. Section 8.1 provides the standards for the design of Rail Trail Connections.
- E. All alleys shall be 26 feet wide.
- F. At all alley intersections, a 10-foot curb radius shall be provided.
- G. Development sites are required along the edges of the Rail Trail, Rail Trail Connections, and all streets.
- H. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- I. All development sites shall have a minimum depth of 70 feet.
- J. The maximum size of any one development site shall be 1.75 acres.

Figure 4-6: Example Subdivision of Block Type E



Development Sites 2, 3, 4, and 5 may be merged in various combinations to create a larger development site for stacked flat buildings. Development sites that may be merged include 2 and 3; 4 and 5; and 2, 3, 4, and 5.



Block Type F

Block Type F is currently developed with apartment buildings that are privately owned. A small strip of land that is owned by the City of Los Banos is located between the apartment property and the existing Rail Trail. This block is not expected to be redeveloped in the near future. However, if the property is redeveloped, the City and the private property owner are encouraged to form a public-private partnership for the redevelopment of the block. As an incentive to forming this partnership, the combination of the City owned land and the private property would create a larger development site for a redevelopment project.

If this block is redeveloped, it should be subdivided based on the subdivision standards for Block Type A, with the exception of the 50-foot minimum depth standard for a building type. Given the relatively narrow distance between the Rail Trail and G Street on this block, the minimum depth of the development site shall be 40 feet.

Block Type G

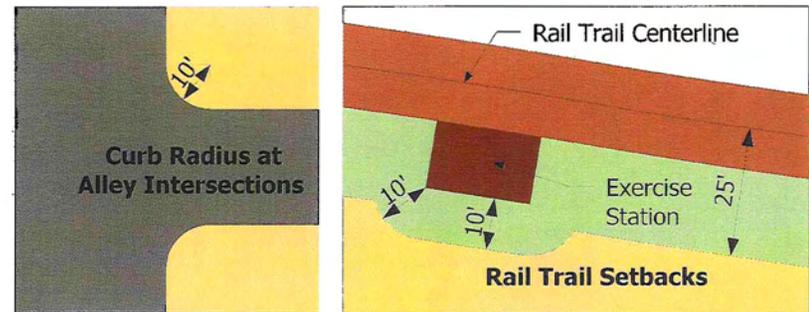
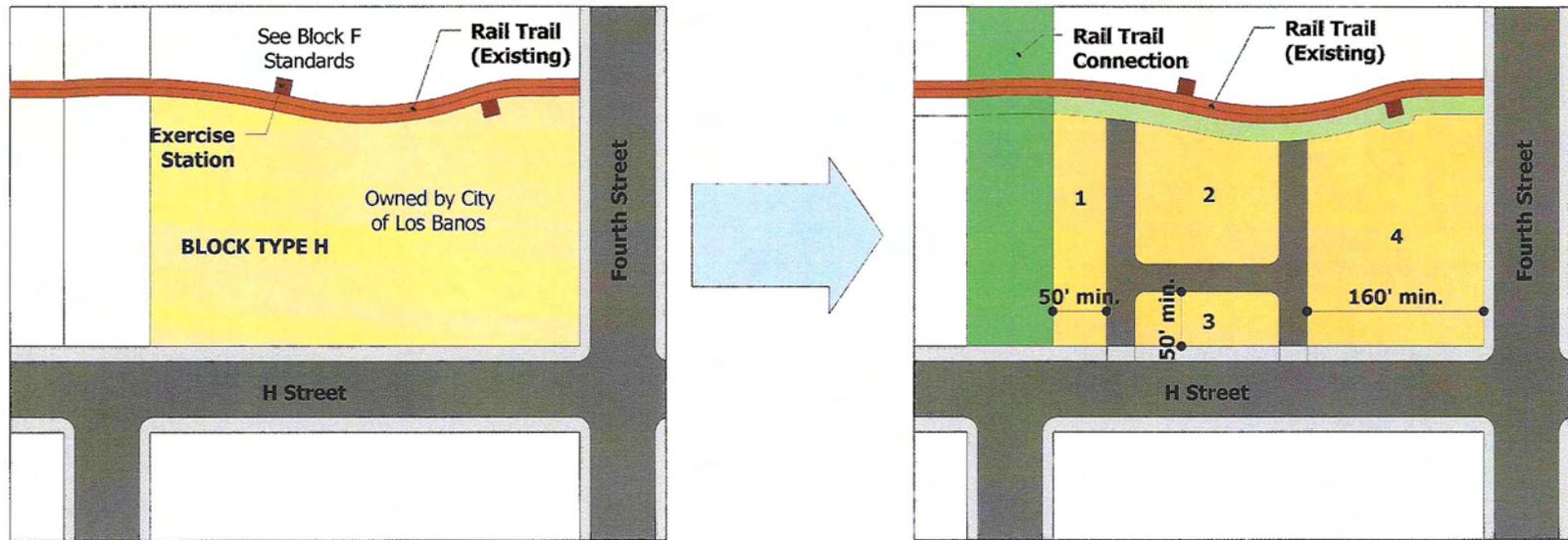
Standards are not provided for Block Type G. These blocks are intended for public/civic uses and do not require further subdivisions.

Block Type H

The block identified as Block Type H on Figure 4-1 shall be subdivided into development sites based on the following standards:

- A. The block shall be subdivided into four development sites and a rear alley network. The specific location, design, and size of the development sites and the alley network may vary provided that the standards below are achieved.
- B. Development sites are required along the edges of all sides of the block (see Figure 4-7)
- C. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- D. The alley network shall have an “H” shaped configuration and provide access to the back of all development sites (see Figure 4-7). The alley network shall connect to H Street at two locations. Alleys shall not connect to Fourth Street
- E. All alleys shall be 26-feet wide.
- F. At all alley intersections, a 10-foot curb radius shall be provided
- G. Development sites shall have a minimum depth of 50 feet. Larger depths are required to accommodate certain building types, such as Courtyard Flats (70 feet), Rear Yard Townhouses (80 feet), Cottage Houses (80 feet), and the Bus Depot (160 feet)
- H. A Rail Trail Connection shall be established along the former right-of-way of Third Street between H Street and the existing Rail Trail. The Rail Trail Connection shall be 80 feet wide. Section 8.1 provides the standards for the design of Rail Trail Connections.

Figure 4-7: Example Subdivision of Block Type H

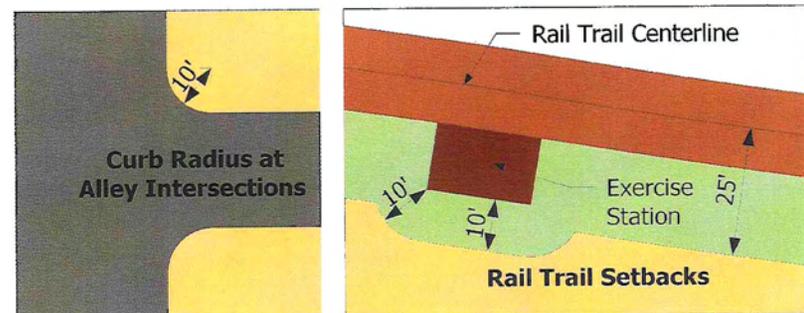
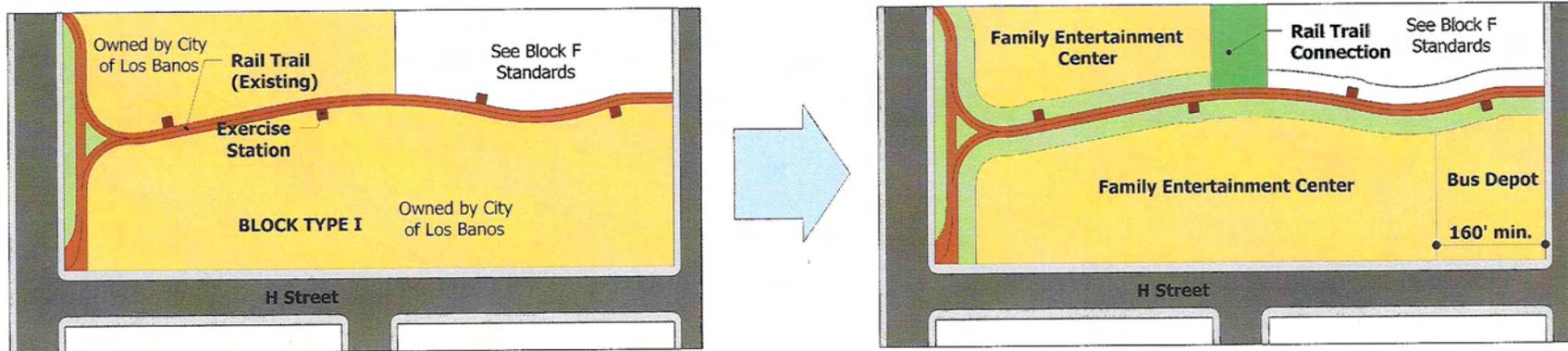


Block Type I

The blocks identified as Block Type I on Figure 4-1 shall be subdivided into development sites based on the following standards if the Family Entertainment Center alternative is pursued:

- A. The blocks shall be subdivided to create a development site for the Bus Depot (located at the northwest corner of H Street and Fourth Street) and two development sites for the Family Entertainment Center (one to the north of the Rail Trail and one to the south of the Rail Trail)
- B. The edges of all development sites shall be setback at least 25 feet from the centerline of the existing Rail Trail and 10 feet from the edges of all existing exercise stations along the Rail Trail.
- C. The Bus Depot development site shall have a minimum width (as measured along H Street) of 160 feet
- D. A Rail Trail Connection shall be established along the former right-of-way of Third Street between G Street and the existing Rail Trail. The Rail Trail Connection shall be 80 feet wide. Section 8.1 provides the standards for the design of Rail Trail Connections.

Figure 4-8: Example Subdivision of Block Type I



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5.0 Site and Building Standards

As described in Section 3.0 (Building Types and Regulating Plan), a variety of building types are allowed in the Rail Trail Corridor. The Regulating Plan (see Figure 3-1) identifies locations where each building type is allowed. The development standards in this section are broken down by each of the building types identified in the Regulating Plan:

- Mixed-Use Buildings
- Commercial Buildings
- Live-Work Buildings
- Stacked Flat Buildings
- Courtyard Flat Buildings
- In-Line Townhouse Buildings
- Cluster Townhouse Buildings
- Rear Yard Townhouse Buildings
- Cottage House Buildings
- Family Entertainment Center Buildings

The following types of standards are provided for each of the above building types:

- Pedestrian Connections
- Site Design
- Mass and Height
- Window Fenestration and Frontages
- Projections
- Building Uses
- Parking Requirements

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5.1 Mixed-Use Buildings

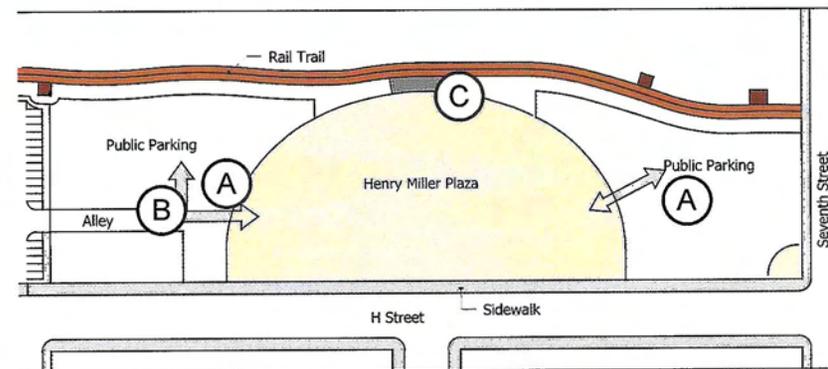
Mixed-Use Buildings are two to three story buildings designed for a mix of commercial and residential uses. The ground floor generally contains storefronts for retail stores, restaurants, and cafes. Upper floors generally contain apartments, condominiums, and/or office space.



Example of a mixed-use building

Pedestrian Connections

- A. If public parking facilities are provided within a development site, a walkway between the public parking facility and Henry Miller Plaza is required. The walkway shall be located within an internal building hallway or a pedestrian cut-through (or paseo).
- B. If an alley is provided between the two development sites to the west of Henry Miller Plaza, a walkway shall connect the end of the alley to Henry Miller Plaza. The walkway shall be located within an internal building hallway or a pedestrian cut-through (or paseo).
- C. A walkway connecting the Rail Trail and Henry Miller Plaza is required.



Site Design (see illustration on the following page)

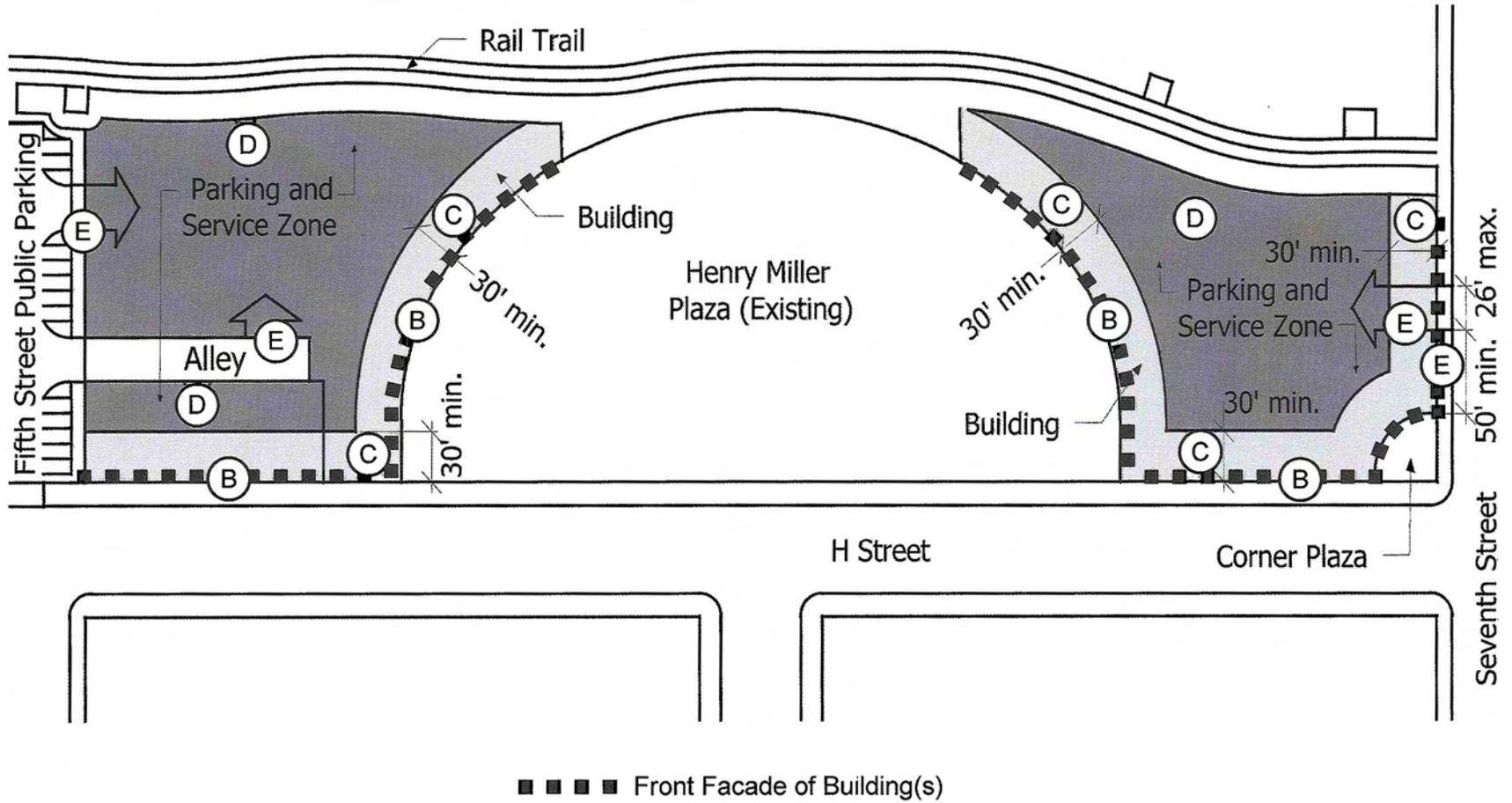
- A. Building Setbacks: No setbacks are required.
- B. Build-to-Line (BTL) The BTL shall be located at a zero foot setback along lot lines that are adjacent to H Street, Henry Miller Plaza, the Corner Plaza, and Seventh Street. Front facades shall occupy 100% of each development site's BTL.
- C. The minimum depth of the mixed use buildings constructed along the BTL shall be 30 feet.
- D. Parking and Service Zone: This zone includes the portions of the development site that are setback at least 30 feet from the BTL. If provided, parking structures, surface parking, podium parking levels, trash/recycling storage areas, delivery zones, and service areas shall be located in this zone. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All external trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the mixed-use building.
- E. Access to parking shall be provided from an alley, the Fifth Street Public Parking Lot, or Seventh Street. Driveway access from H Street is prohibited. Driveways shall be located at least 50 feet from the edge of the corner plaza. Upper floors of buildings may extend over driveways. The maximum width of a driveway shall be 26 feet. Access to and from Seventh Street will be limited to right-in and right-out turning movements.

- F. Utility meters shall be located within equipment enclosures that are located within a parking structure or podium parking level, or along a rear or side facade. Utility meters are prohibited along front facades.
- G. Pedestrian cut-throughs (or paseos) that provide access between sidewalks, parking facilities, the Rail Trail, and public plazas are allowed throughout the site. If provided, cut-throughs (or paseos) shall be 10 to 20 feet wide. All cut-throughs (or paseos) that connect with Henry Miller Plaza shall be covered by the second floor of the building within 20 feet of the BTL along Henry Miller Plaza. All other segments do not have to be covered.



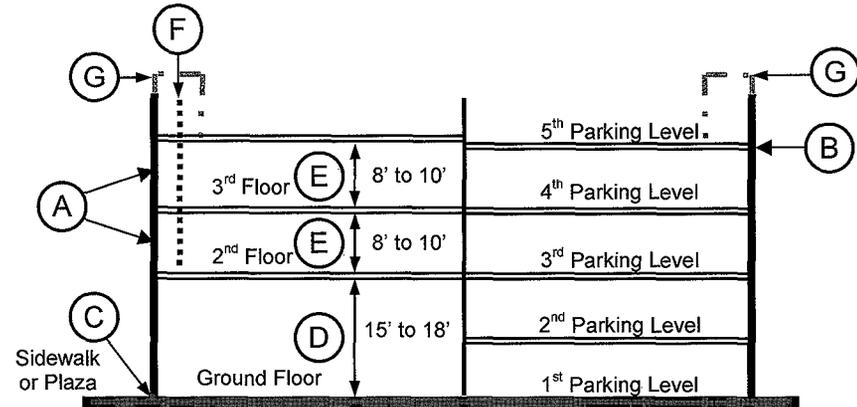
Example of a cut-through that is covered by the second floor of the building.

Site Design



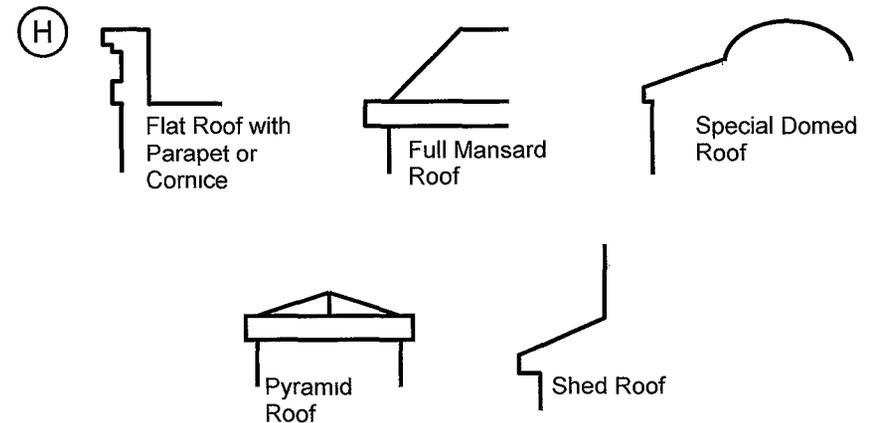
Mass and Height

- A Along the BTL, buildings shall be a minimum of two floors and a maximum of three floors
- B If structured parking is provided, the top parking level shall not be taller than the roof of the adjacent mixed-use building. Parking levels may be visible from the Rail Trail, the Fifth Street Public Parking Lot, and alleys if they are designed in compliance with the standards in Section 6.3 (Parking Structure Facades).
- C The elevation of the ground level floor plate shall be built at or near the elevation of the adjacent sidewalk or plaza so that steps or ramps are not required to enter ground floor businesses
- D Ground floor ceiling heights shall be 15 to 18 feet tall
- E Upper floor ceiling heights shall be 8 to 10 feet tall.
- F Upper floor step-backs are allowed, but not required.
- G. Tower elements may project above roofline of the top floor to provide height variety along the street. A clock tower is required adjacent to the Corner Plaza (see Figure 2-7)



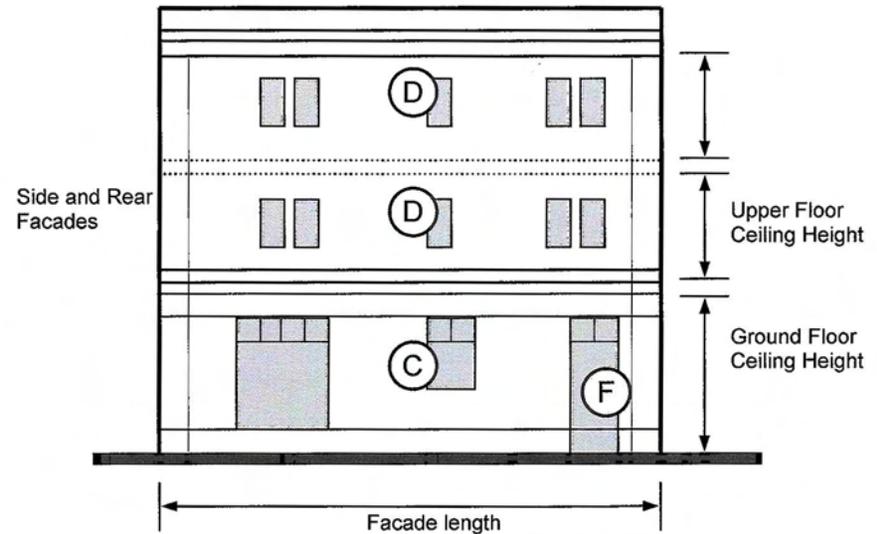
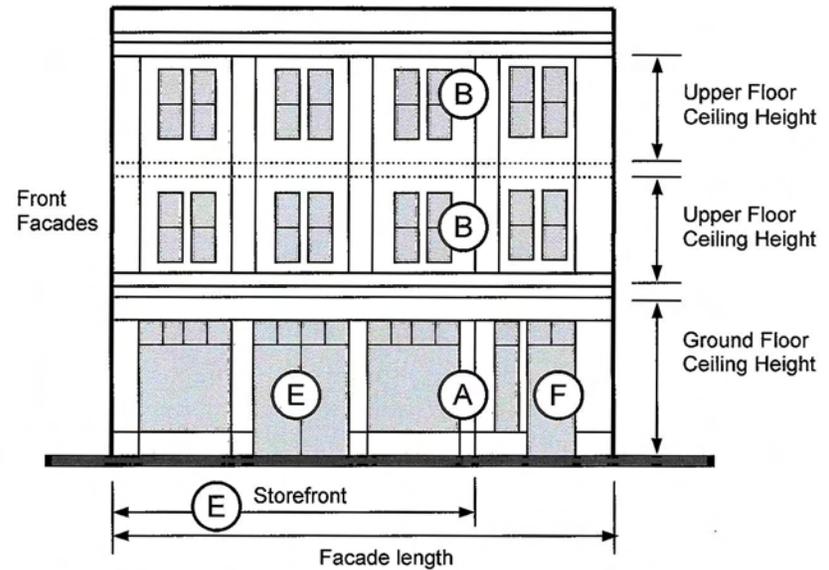
H The following roof forms are allowed.

- Flat Roof with Parapet or Cornice
- Full Mansard Roof
- Special Domed Roof
- Pyramid Roof (only allowed on tower elements)
- Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 60% of the ground floor facade's surface area¹ shall consist of transparent glass windows²
- B. On front facades, at least 20% of each upper floor facade's surface area¹ shall consist of windows²
- C. On rear and side facades, at least 25% of the ground floor facade's surface area¹ shall consist of windows²
- D. On rear and side facades, at least 12% of each upper floor facade's surface area¹ shall consist of windows²
- E. The majority of the front facade shall be occupied by storefronts. A workable storefront entrance door is required at intervals not to exceed 30 feet. The design of storefronts shall comply with the standards in Section 6 10 (Storefronts)
- F. Entrances to upper floor facades shall be provided on a facade that fronts a street, Henry Miller Plaza, or the Corner Plaza.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways, openings on parking structure facades, and parking facility entrances) are considered windows.

Projections

- A. The following building elements may extend from ground floor facades:
- Shade Structures
 - Projecting Signs
- B. The following building elements may extend from upper floor facades:
- Balconies
 - Bay Windows
 - Shade Structures: Window Shades and Trellises
- C. Roof projections and overhangs may project up to 3 feet from facades.
- D. All of the above projections may encroach over an adjacent sidewalk, plaza, walkway, or landscaped surface. The above projections shall not encroach over an adjacent private property
- E. Design standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on ground floor space that has frontage along a front facade

- Retail Uses
- Service Uses

B. The following uses are conditionally permitted on ground floor space that has frontage along a front facade:

- Office Uses

C. The following uses are allowed on ground floor space that does not have frontage along a front facade:

- Retail Uses
- Service Uses
- Office Uses

D. The following uses are allowed on upper floors

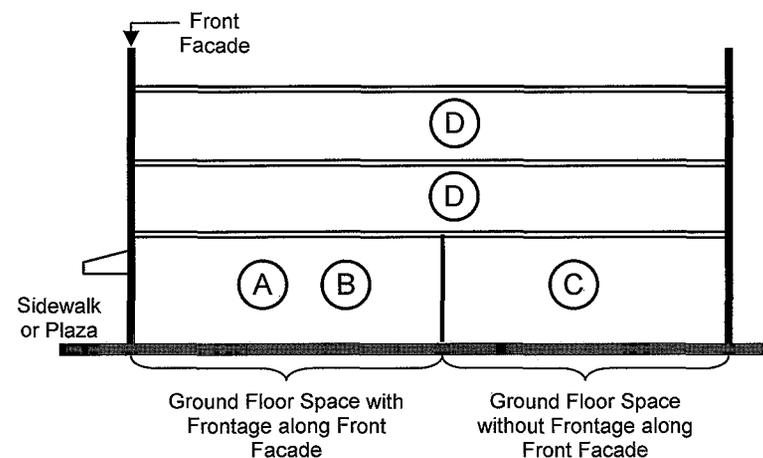
- Residential Uses (apartments and condominiums)
- Home Occupations
- Office Uses
- Service Uses

E. The following uses are conditionally permitted regardless of their location

- Any retail, service, or office use that serves or sells alcohol, occupies a floor area greater than 5,000 square feet, and is open for business between the hours of 9:00 pm and 7 00 am.

F. The following uses are specifically prohibited.

- Adult-oriented or adult-only businesses
- New and used automobile sales
- Drive-thrus
- Gas stations, service stations, auto repair and maintenance facilities
- Business that sell lumber and building materials
- Nurseries
- Lodging uses (hotels, motels, boarding houses, etc.)
- Outdoor storage, manufacturing and light/heavy industrial uses
- Kennels



Parking Requirements

- A. Apartments and Condominiums One parking space is required for all units that are less than 1,000 square feet in size. Two parking spaces are required for all units that are 1,000 square feet or greater in size. All parking shall be provided within the development site. Every unit shall have at least one dedicated parking spot. Off-street guest parking spaces are not required.

- B. Retail, Office, and Service Uses: Four parking spaces are required for every 1,000 square feet of leasable floor space. Fractional space requirements shall be rounded up to the next whole space. At least 75% of the required parking spaces shall be located within the development site or within a shared parking facility located on the same block as the development site. In lieu of providing the remaining 25% of the required spaces, the property owner may pay in-lieu parking fees to fund public parking improvements for the Rail Trail Corridor (see Section 5.13: In-Lieu Parking Program)

- C. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per residential unit and one space per 4,000 square feet of leasable non-residential space. Bike parking for residential units shall be located within parking areas. Bike parking for non-residential uses shall be located within parking areas or along pedestrian connections to the Rail Trail.

5.2 Commercial Buildings

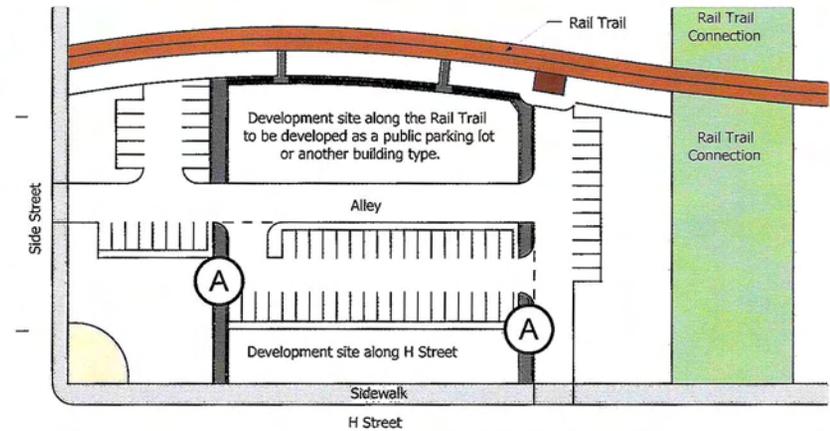
Commercial Buildings are one or two story buildings designed for a mix of commercial uses, including retail, restaurants, cafes, service, and office uses



Example of a commercial building

Pedestrian Connections

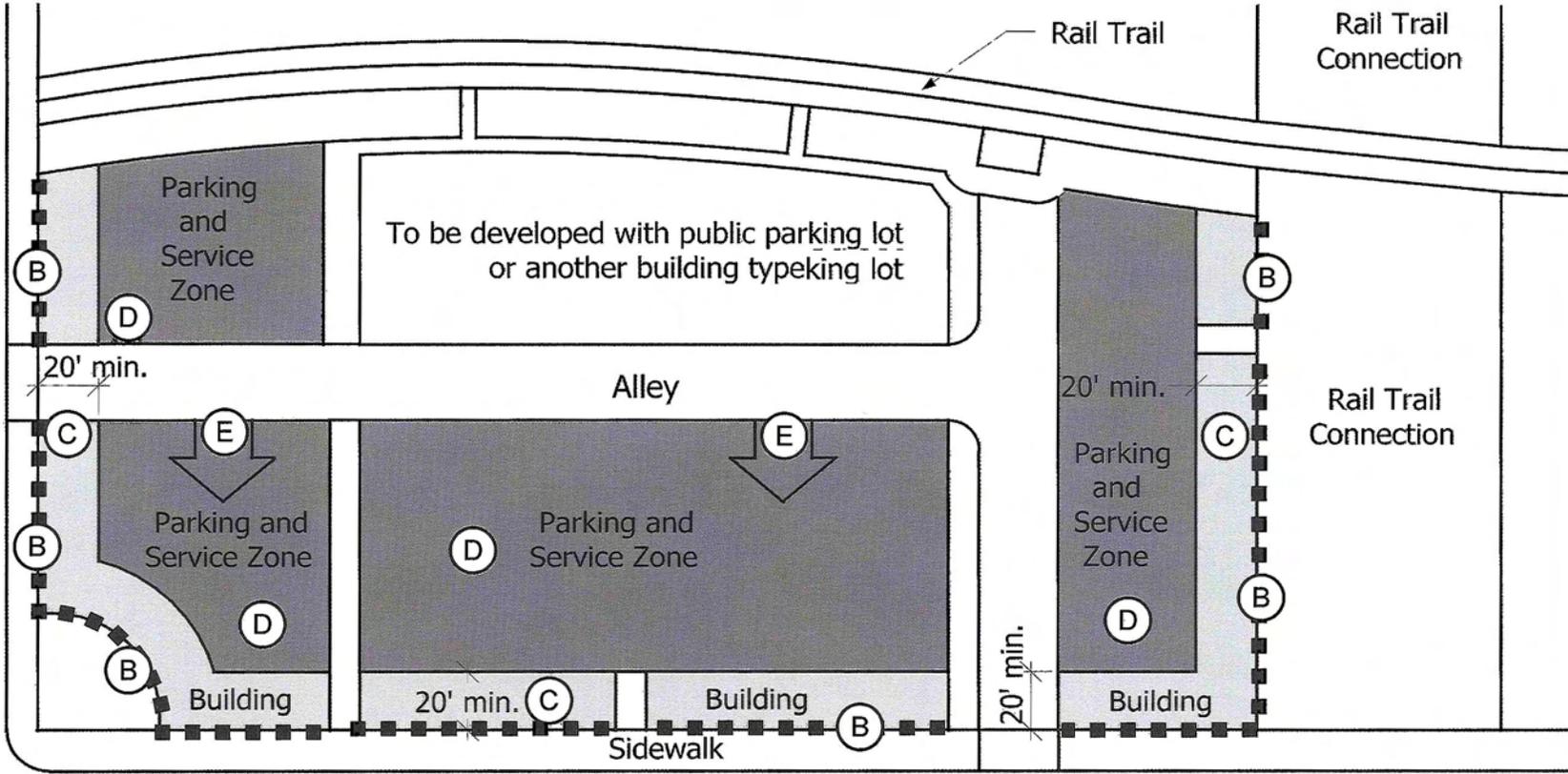
- A. The development site along H Street shall contain at least two walkways that connect the sidewalk on H Street to the walkways on the development site along the Rail Trail.
- B. All walkways shall be a minimum of 6 feet wide



Site Design (see Illustration on the following page)

- A. Building Setbacks: No setbacks are required
- B. Build-to-Line (BTL): The BTL shall be located within zero to 5 feet of lot lines that are adjacent to streets, the Rail Trail Connections, and Corner Plaza. There is no specific BTL for property lines along the Rail Trail. Front facades shall occupy at least 75% of each development site's BTL.
- C. The minimum depth of the commercial buildings constructed along the BTL shall be 20 feet.
- D. Parking and Service Zone: This zone includes the portions of the development site that are setback at least 20 feet from the BTL. If provided surface parking, tuck-under parking, trash/recycling storage areas, delivery zones, and service areas shall be located in this zone. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All external trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the commercial buildings.
- E. Access to parking shall be provided from the development site's rear alley.
- F. Utility meters shall be located within equipment enclosures that are located along a rear or side facade or within the parking and service zone. Utility meters are prohibited along front facades.
- G. The second floor of buildings may extend over alley entrances and rear parking areas.
- H. Pedestrian cut-throughs (or paseos) that provide access between sidewalks, parking facilities, the Rail Trail, and public plazas are allowed throughout the site. If provided, cut-throughs (or paseos) shall be 10 to 20 feet wide. Cut-throughs may be covered by the second floor of the building.

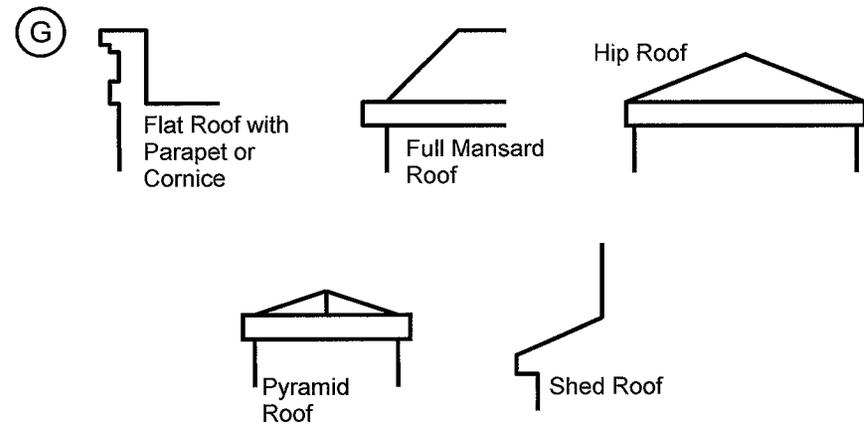
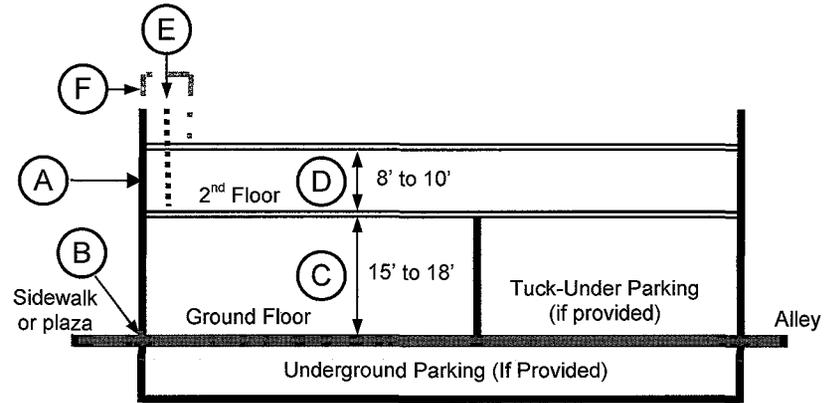
Site Design



■ ■ ■ ■ Front Facade of Building(s)

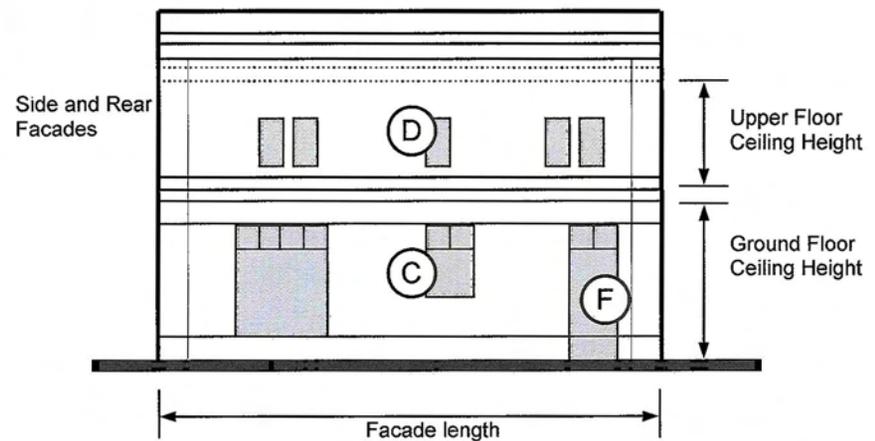
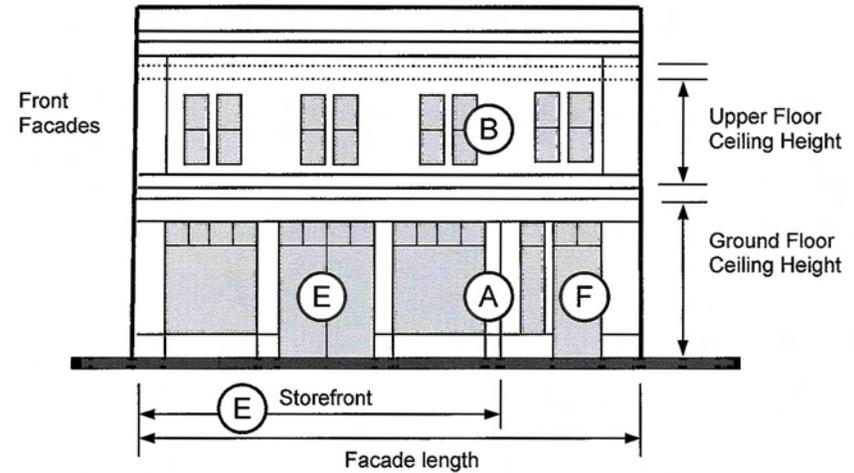
Mass and Height

- A. Buildings shall have a maximum height of two floors.
- B. The elevation of the ground level floor plate shall be built at or near the elevation of the adjacent sidewalk or plaza so that steps or ramps are not required to enter ground floor businesses
- C. Ground floor ceiling heights shall be 15 to 18 feet tall.
- D. Upper floor ceiling heights shall be 8 to 10 feet tall.
- E. Upper floor step-backs are allowed.
- F. Tower elements may project above roofline of the top floor to provide height variety along the street
- G. The following roof forms are allowed
 - Flat Roof with Parapet or Cornice
 - Full Mansard Roof
 - Hip Roof
 - Pyramid Roof (only allowed on tower elements)
 - Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 50% of the ground floor facade's surface area¹ shall consist of windows²
- B. On front facades, at least 15% of each upper floor facade's surface area¹ shall consist of windows²
- C. Along rear and side facades, at least 20% of the ground floor facade's surface area¹ shall consist of windows². This standard does not apply if tuck-under parking is provided.
- D. Along rear and side facades, at least 10% of each upper floor facade's surface area¹ shall consist of windows²
- E. The majority of the front facade shall be occupied by storefronts. A workable storefront entrance door is required at intervals not to exceed 30 feet. The design of storefronts shall comply with the standards in Section 6 10 (Storefronts)
- F. Entrances to upper floor facades shall be provided on a facade that fronts a street, Corner Plaza, Rail Trail, or Rail Trail Connection
- G. Fences and walls are prohibited in front of a front facade. All allowed fences and walls shall be limited to a height of 6 feet.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.
² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways and parking facility entrances) are considered windows.

Projections

- A. The following building elements may extend from ground floor facades:
 - Shade Structures
 - Projecting Signs
- B. The following building elements may extend from upper floor facades:
 - Balconies
 - Bay Windows
 - Shade Structures: Window Shades and Trellises
- C. Roof projections and overhangs may project up to 3 feet from facades.
- D. All of the above projections may encroach over an adjacent sidewalk, plaza, walkway, or landscaped surface. The above projections shall not encroach over an adjacent private property
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on the ground floor of the building.

- Retail Uses
- Service Uses
- Office Uses

B. The following uses are allowed on upper floors of the building:

- Office Uses
- Service Uses

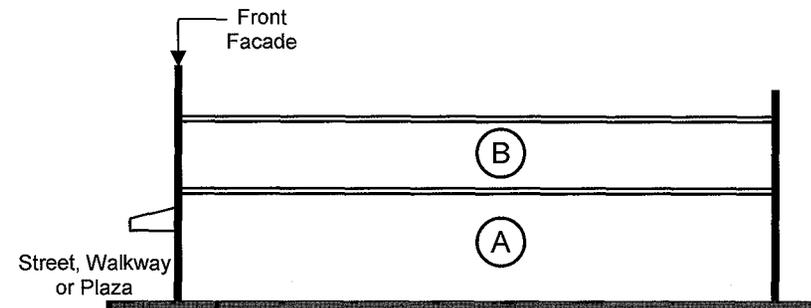
C. The following uses are conditionally permitted regardless of their location:

- Any retail, service, or office use that serves or sells alcohol, occupies a floor area greater than 5,000 square feet, and is open for business between the hours of 9:00 pm and 7:00 am.

D. The following uses are specifically prohibited:

- Residential uses
- Adult-oriented or adult-only businesses
- New and used automobile sales
- Drive-thrus
- Gas stations, service stations, auto repair and maintenance facilities
- Business that sale lumber and building materials
- Nurseries

- Lodging uses (hotels, motels, boarding houses, etc)
- Outdoor storage, manufacturing and light/heavy industrial uses
- Kennels

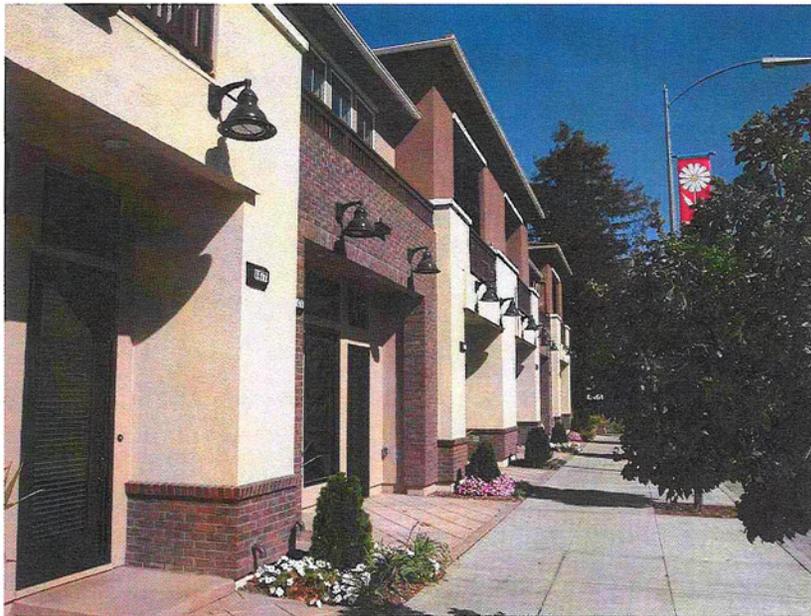


Parking Requirements

- A. Retail, Office, and Service Uses: Four parking spaces are required for every 1,000 square feet of leasable floor space. Fractional space requirements shall be rounded up to the next whole space.
- B. At least 75% of the required parking spaces shall be located within the development site or within a shared parking facility located on the same block as the development site. In lieu of providing the remaining 25% of the required spaces, the property owner may pay in-lieu parking fees to fund public parking improvements for the Rail Trail Corridor (see Section 5.13 In-Lieu Parking Program).
- C. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per 4,000 square feet of leasable non-residential space. Bike parking shall be located within parking areas or along pedestrian connections to the Rail Trail.

5.3 Live-Work Buildings

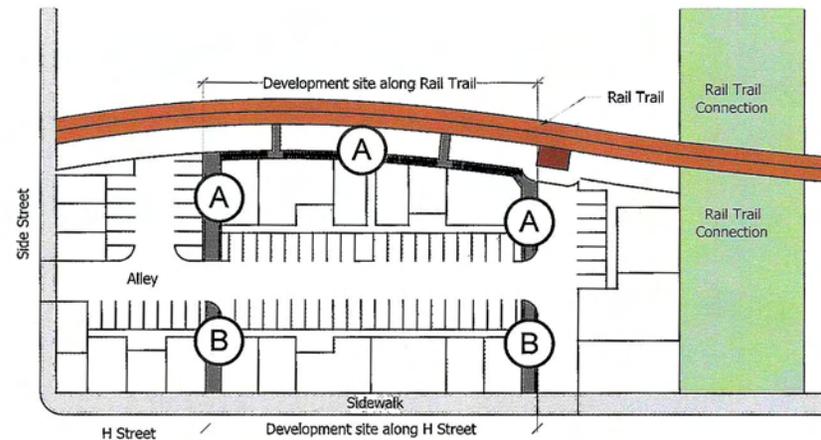
Live-Work Buildings are one and two story buildings that contain units in which the owner can both live and operate certain types of office and service businesses. The space designed for office and service businesses may need to be designed to commercial building code requirements depending on the types of businesses that would be allowed by the development.



Example of live-work building

Pedestrian Connections

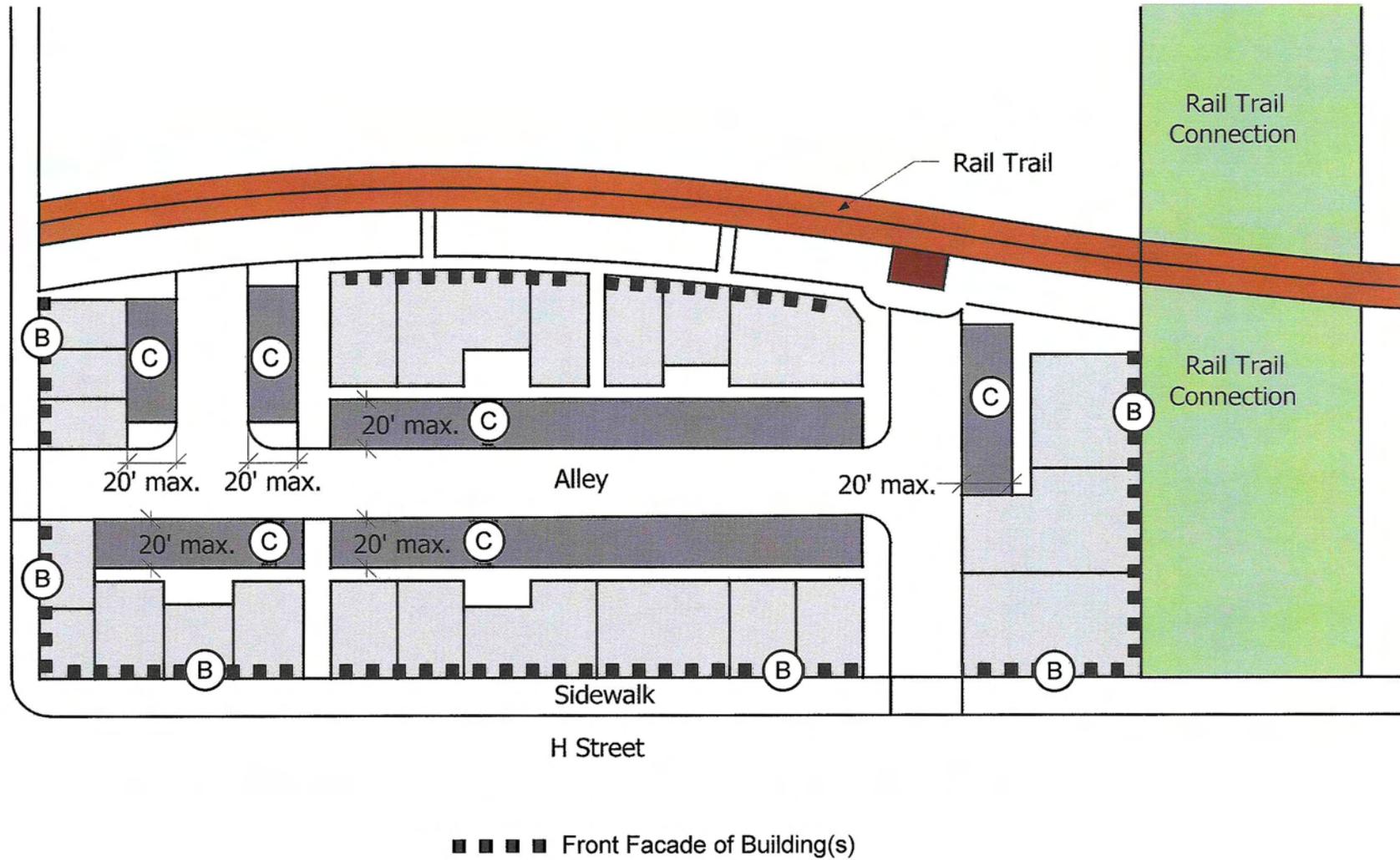
- A. The development site along the Rail Trail shall contain a common walkway along the front of the units that connects to the rear corners of the development site and to the Rail Trail (the front of the units face the Rail Trail).
- B. The development site along H Street shall contain at least two walkways that connect the sidewalk on H Street to the walkways on the development site along the Rail Trail.
- C. All walkways shall be a minimum of 6 feet wide.



Site Design (see Illustration on the following page)

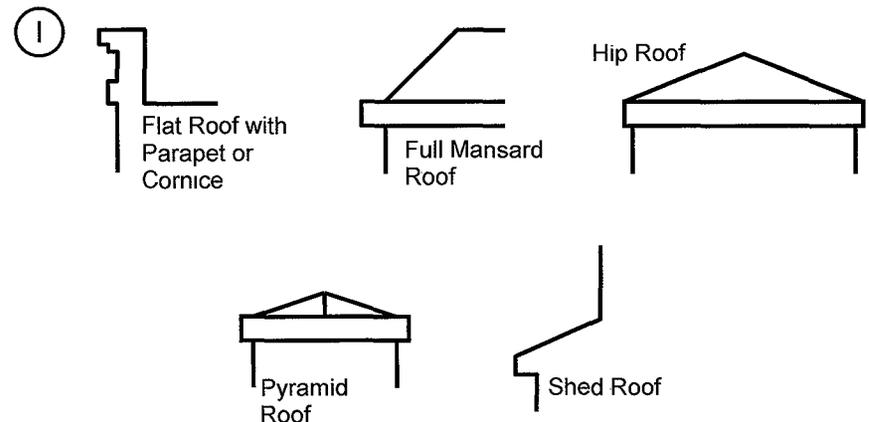
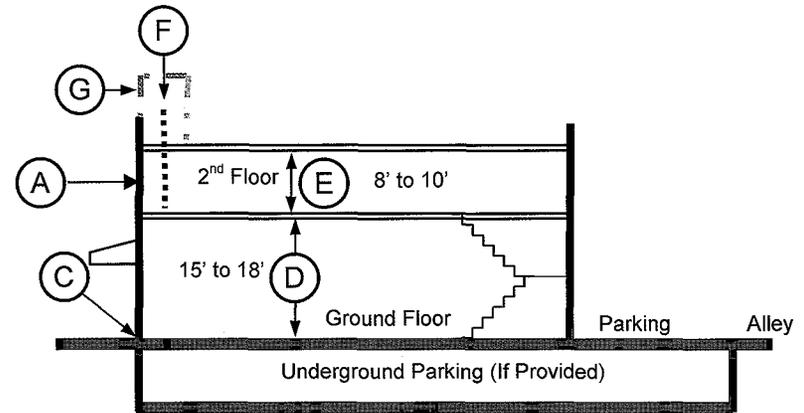
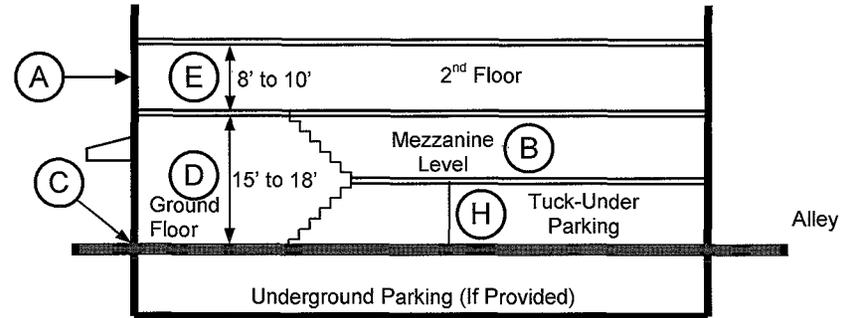
- A. Building Setbacks: No setbacks are required.
- B. Build-to-Line (BTL): The BTL shall be located within zero to 5 feet of lot lines that are adjacent to Streets, Corner Plazas, and Rail Trail Connections. For the development sites that face the Rail Trail, there is no specific BTL. However, units on these sites shall front the Rail Trail. Front facades shall be built along at least 75% each BTL.
- C. Parking and Trash/Recycling Storage: Parking spaces and trash/recycle storage areas shall be located behind the units within a zone that extends no more than 20 feet from the alley. Access to parking shall be provided from the adjacent alleys. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All external trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the live work units.
- D. Utility meters shall be located within equipment enclosures that are located along a rear or side facade. Utility meters are prohibited along front facades.
- E. The second floor of buildings may extend over alley entrances and rear parking areas.
- F. Pedestrian cut-throughs (or paseos) that provide access between sidewalks, parking facilities, the Rail Trail, and public plazas are allowed throughout the site. If provided, cut-throughs (or paseos) shall be 10 to 20 feet wide. Cut-throughs may be covered by the second floor of the building.

Site Design



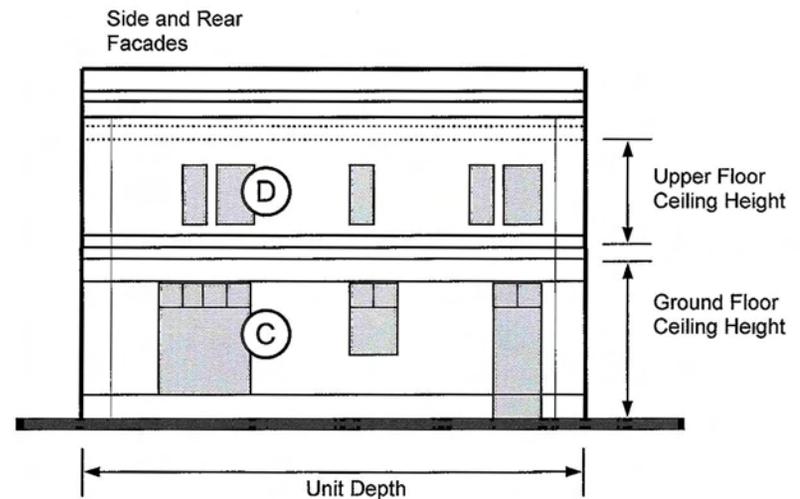
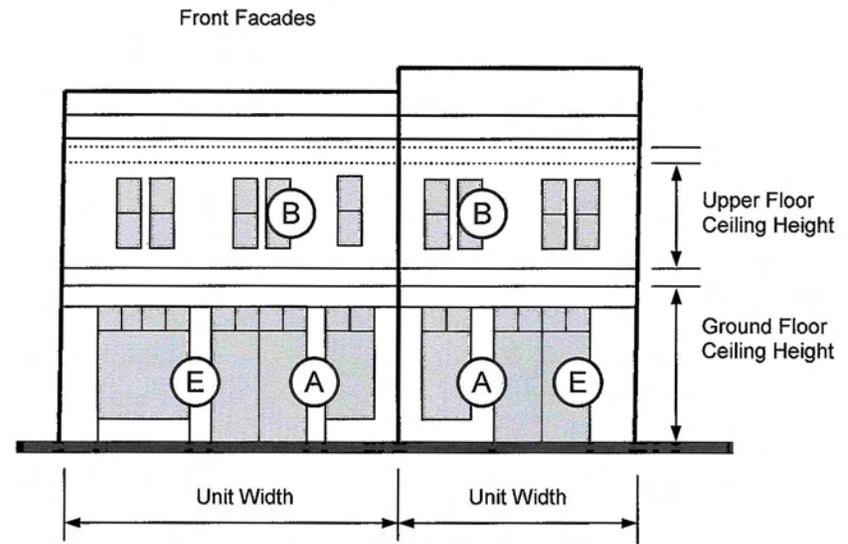
Mass and Height

- A. Each live-work unit shall have a maximum height of two floors.
- B. A mezzanine level is allowed to create an elevated loft space that overlooks the ground floor. The mezzanine level shall not be counted as a floor
- C. The elevation of the ground level floor plate shall be built at or near the elevation of the adjacent sidewalk or plaza so that steps or ramps are not required to enter ground floor businesses
- D. Ground floor ceiling heights shall be 15 to 18 feet tall.
- E. Upper floor ceiling heights shall be 8 to 10 feet tall.
- F. Upper floor step-backs are allowed.
- G. Tower elements may project above roofline of the top floor to provide height variety along the street.
- H. The second floor and/or mezzanine level may extend over a level of tuck-under parking.
- I. The following roof forms are allowed.
 - Flat Roof with Parapet or Cornice
 - Full Mansard Roof
 - Hip Roof
 - Pyramid Roof (only allowed on tower elements)
 - Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 50% of the ground floor facade's surface area¹ shall consist of windows²
- B. On front facades, at least 15% of each upper floor facade's surface area¹ shall consist of windows²
- C. On side and rear facades, at least 20% of the ground floor facade's surface area¹ shall consist of windows². This standard does not apply if tuck-under parking is provided.
- D. On side and rear facades, at least 10% of each upper floor facade's surface area¹ shall consist of windows²
- E. Each unit shall have a storefront on the front facade. The design of Storefronts shall comply with the standards in Section 6.10 (Storefronts).
- F. Fences and walls are prohibited in front of a front facade. All allowed fences and walls shall be limited to a height of 6 feet



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways and parking facility entrances) are considered windows

Projections

- A. The following building elements may extend from ground floor facades:
- Shade Structures
 - Projecting Signs
- B. The following building elements may extend from upper floor facades.
- Balconies
 - Bay Windows
 - Shade Structures: Window Shades and Trellises
- C. Roof projections and overhangs may project up to 3 feet from facades.
- D. All of the above projections may encroach over an adjacent sidewalk, plaza, walkway, or landscaped surface. The above projections shall not encroach over an adjacent private property.
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on the ground floor and mezzanine level of a live-work unit:

- Residential Uses
- Home occupations in compliance with Section 9-3 228 of the Zoning Ordinance.
- Service Uses that are limited to one employee in addition to the residents of the unit.
- Office Uses that are limited to one employee in addition to the residents of the unit

B. The following uses are allowed on the second floor of the building:

- Residential Uses
- Home occupations in compliance with Section 9-3 228 of the Zoning Ordinance.

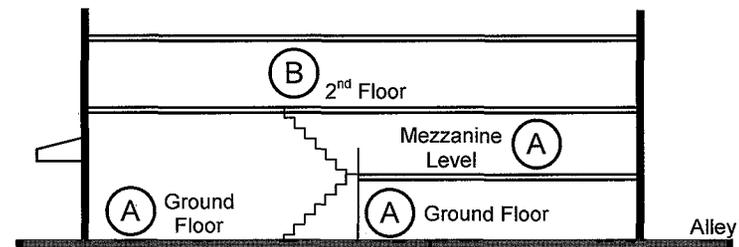
C. The following uses are conditionally permitted regardless of their location

- Any service or office use that is open for business between the hours of 9 00 pm and 7 00 am.

D. The following uses are specifically prohibited:

- Adult-oriented or adult-only businesses
- Drive-thrus
- Gas stations, service stations, auto repair and maintenance facilities
- Lodging uses (hotels, motels, boarding houses, etc)
- Health clubs and gyms

- Movie theaters
- Pet grooming
- Laundry and dry cleaning
- Veterinary clinics
- Medical services (doctors, chiropractors, dentists and orthodontists offices, medical laboratories, etc.)
- Outdoor storage, manufacturing and light/heavy industrial uses
- Kennels
- Blue printing/copy centers
- Retail



Parking Requirements

- A. Two parking spaces are required for every live-work unit. Every unit shall have at least one dedicated parking spot that is located within the development site or within a shared parking facility located on the same block as the development site.
- B. Off-street guest parking spaces are not required.
- C. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per unit. Bike parking shall be located within parking areas or along pedestrian connections to the Rail Trail.

5.4 Stacked Flat Buildings

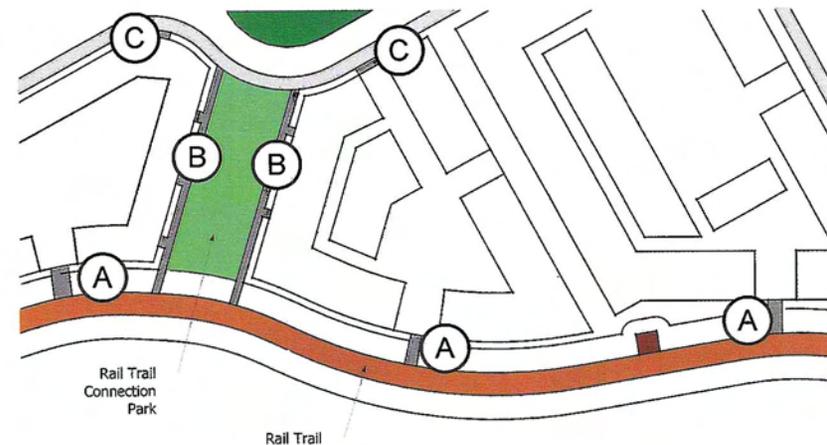
Stacked Flats are two to three story buildings designed with stacked residential units (condominiums or apartments). Parking is provided in a podium parking level or a surface parking lot that is surrounded by the residential units.



Example of stacked flat buildings

Pedestrian Connections

- A. Each development site along the Rail Trail shall include at least one pedestrian walkway that connects the buildings on the site to the Rail Trail.
- B. Development sites along the Rail Trail Connection shall include a walkway along the park that provides pedestrian access between the Rail Trail and the sidewalk on the Neighborhood Park Street. At least one access point shall connect this walkway to the adjacent building.
- C. Walkways shall be provided to connect sidewalks and walkways to building entrances.
- D. All walkways shall be a minimum of 5 feet wide.



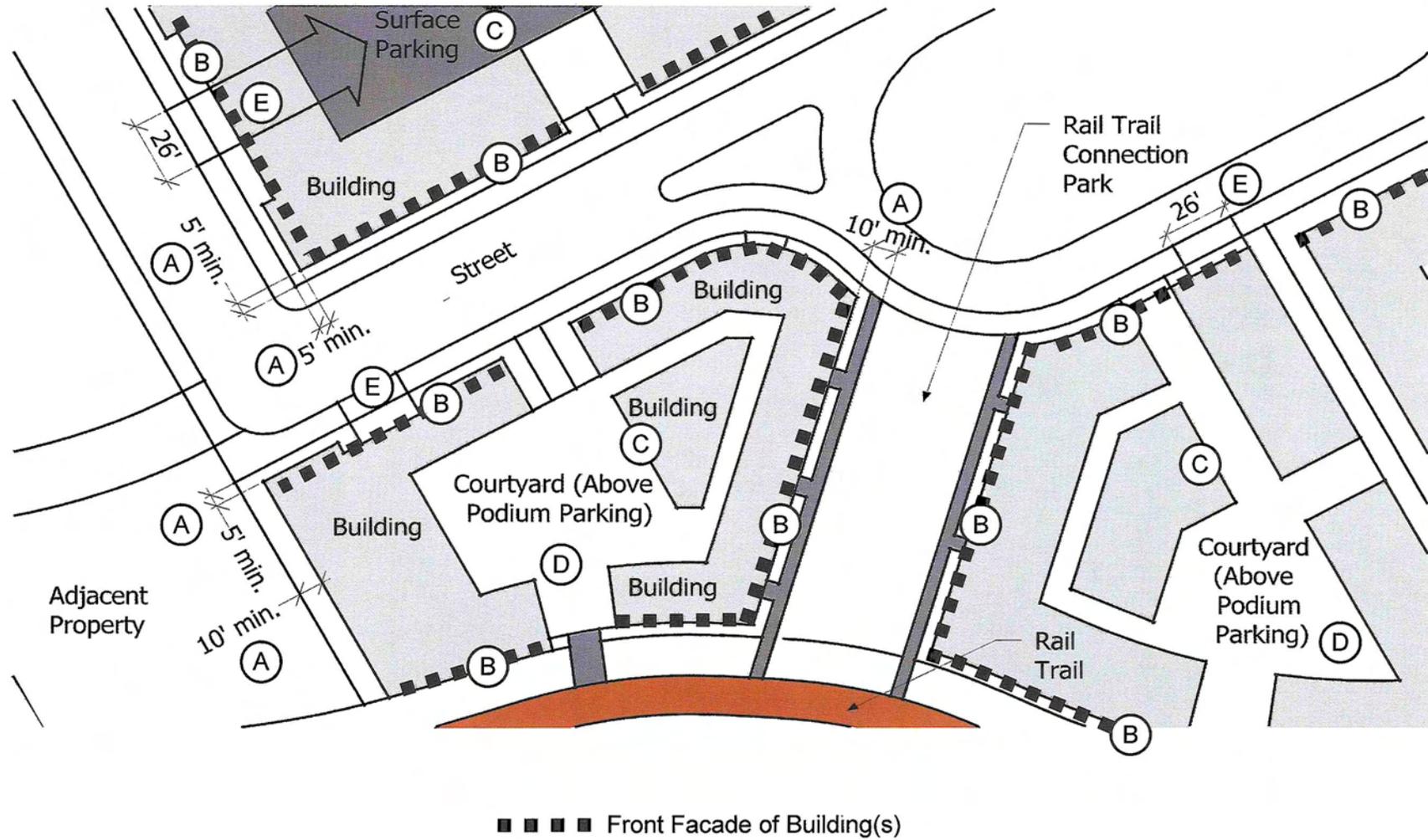
Site Design (see Illustration on the following page)**A. Building Setbacks:** Buildings shall be setback at least:

- 5 feet from front and side property lines that are adjacent to a street or alley
- 10 feet from side property lines that are shared with an adjacent development site or the Rail Trail Connection.

No setbacks are required from property lines along the Rail Trail.

B. Build-to-Line (BTL). The BTL shall be located within 5 to 10 feet of lot lines that are adjacent to streets and the Rail Trail Connection. There is no specific BTL for property lines along the Rail Trail. However, a front facade shall face the Rail Trail. Front facades shall be built along at least 75% each BTL.**C. Parking and Trash/Recycling Storage:** Parking and trash/recycling storage areas shall be located within a podium parking level (located beneath the building) or a surface parking lot. Surface parking lots are not allowed along streets, the Rail Trail, and the Rail Trail Connection (i.e. buildings must be located between the surface parking lot and streets, the Rail Trail, and Rail Trail Connections). Surface parking is allowed along alleys. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All external trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the stacked flat building.**D.** If podium parking is used, the development site shall include at least one central courtyard that is located above the parking level. No dimension of the courtyard shall be less than 26 feet in length. The courtyard is not required if surface parking used.**E.** Access to parking facilities shall be provided by two-way driveways that connect to a street or alley. The maximum width of a driveway shall be 26 feet. Upper floors of the building may extend over driveway entrances.**F.** All utility meters shall be located within equipment enclosures that are located within the podium parking level or a rear facade along the central parking lot.

Site Design



Mass and Height

A. Buildings shall have a minimum of two and a maximum of three floors. A level of podium parking is allowed beneath the building and shall not be counted as a floor

B. If podium parking is provided beneath a segment of the building, the elevation of the ground level floor plate shall be within 36 to 72 inches above the grade of all adjacent sidewalks. If podium parking is not located beneath a segment of the building, the elevation of the ground level floor plate shall be 36 to 48 inches above the grade of the all adjacent sidewalks.

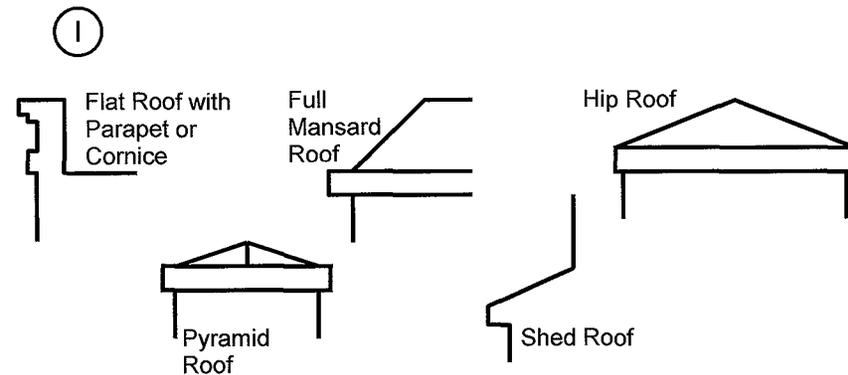
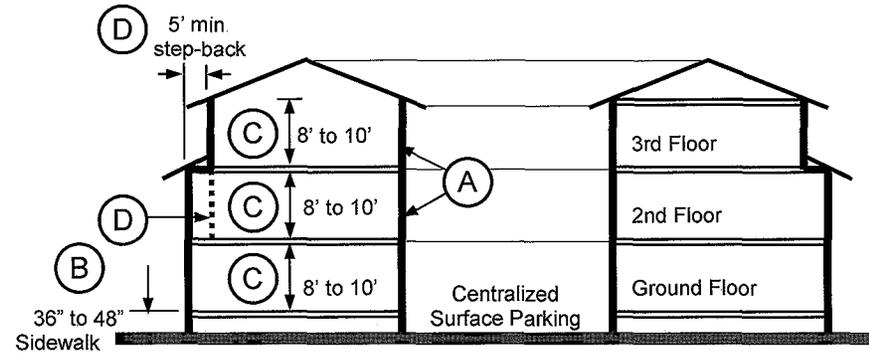
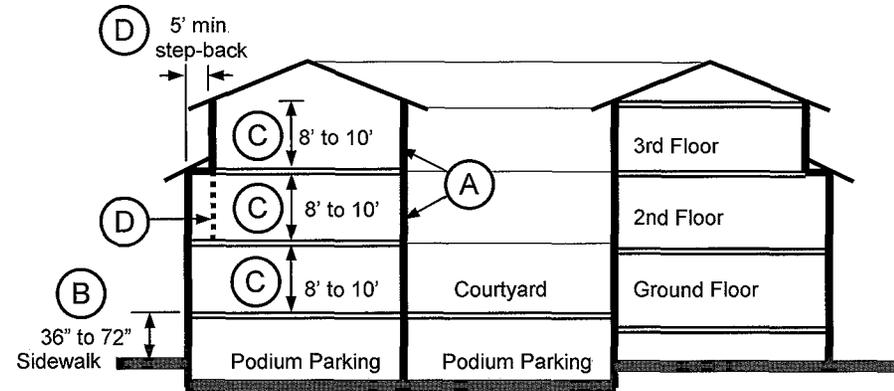
C. Ceiling heights shall be 8 to 10 feet tall. Vaulted ceilings are allowed on the top floor if the ceiling vault starts within 8 to 10 feet of the floor

D. Upper floor step-backs are allowed. If three floors of housing are provided, the third floor shall step-back at least 5' from the second floor

E. Tower elements may project above roofline of the top floor to provide height variety along the street.

F. The following roof forms are allowed

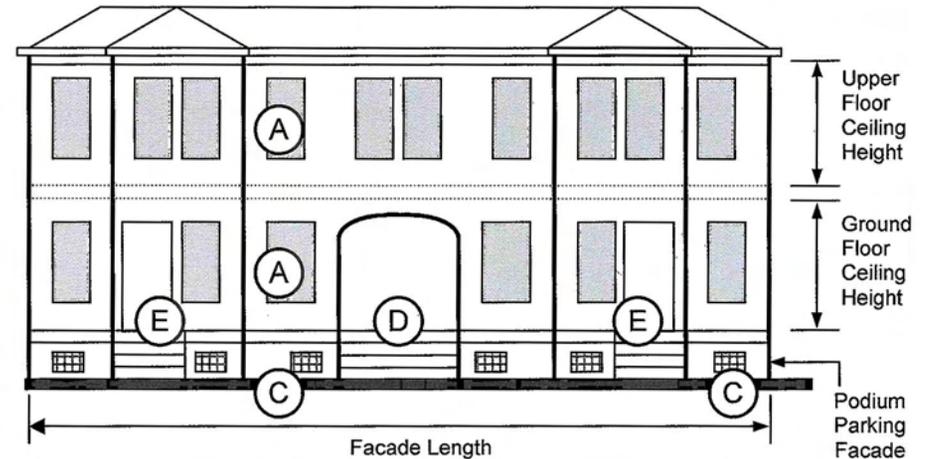
- Flat Roof with Parapet or Cornice
- Full Mansard Roof
- Hip Roof
- Pyramid Roof (only allowed on tower elements)
- Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

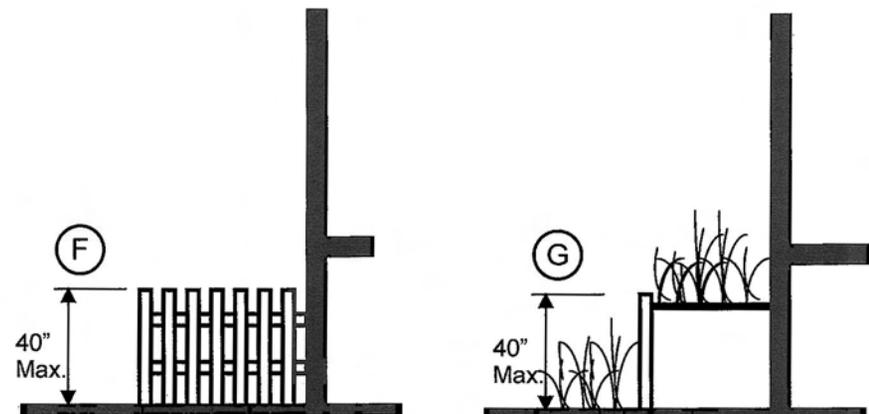
- A. On front facades, at least 15% of the each floor facade's surface area¹ shall consist of windows²
- B. On rear and side facades, at least 10% of the each floor facade's surface area¹ shall consist of windows²
- C. Podium parking facades that extend above the ground surface shall be designed as an attractive base to the building that consists of a solid surface with window openings in compliance with Section 6.5 (Podium Parking).
- D. At least one common building entrance shall be provided on a front facade adjacent to a street. The common entrance may provide access into a common lobby or an internal courtyard.
- E. Private entrances to ground-floor units may be designed with stoops in compliance with the applicable standards in Section 6.14 (Front Porches and Stoops). Stoops may be recessed into the facade or may project out from the facade and encroach into the setback.
- F. Fences are only allowed within front yards. Fences shall be limited to a height of 40 inches and shall have a minimum transparency of 25%. No other fences are allowed.

- G. Retaining walls are allowed to create a terraced planter along front facades. Retaining wall shall be limited to a height of 40 inches. Landscaping shall be placed in front of the retaining wall.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways, podium parking openings, and parking facility entrances) are considered windows.



Projections

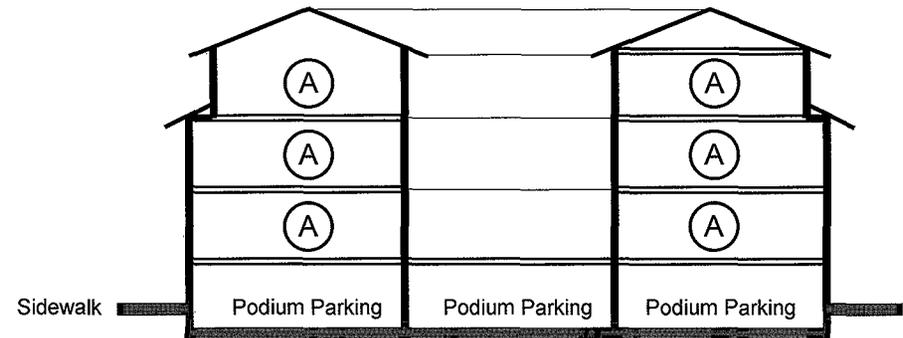
- A. The following building elements may project from ground floor facades and extend into setback areas.
- Shade Structures Window Shades and Trellises
 - Stoops
- B. The following building elements may project from upper floor facades and extend over setback areas:
- Shade Structures: Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas:
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet)
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on all floors of the building.

- Condominium Units
- Apartment Units
- Home occupations in compliance with Section 9-3.228 of the Zoning Ordinance.

B. All other uses are prohibited

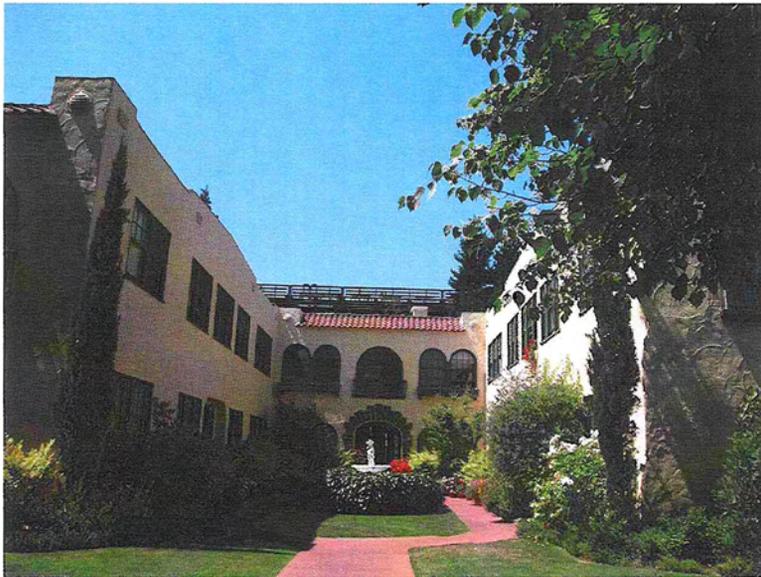


Parking Requirements

- A. Apartments and Condominiums: One parking space is required for all units that are less than 1,000 square feet in size. Two parking spaces are required for all units that are 1,000 square feet or greater in size. All required parking shall be provided within the development site
- B. Every unit shall have at least one dedicated parking spot.
- C. Off-street guest parking spaces are not required.
- D. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per unit. Bike parking shall be located within parking areas.

5.5 Courtyard Flat Buildings

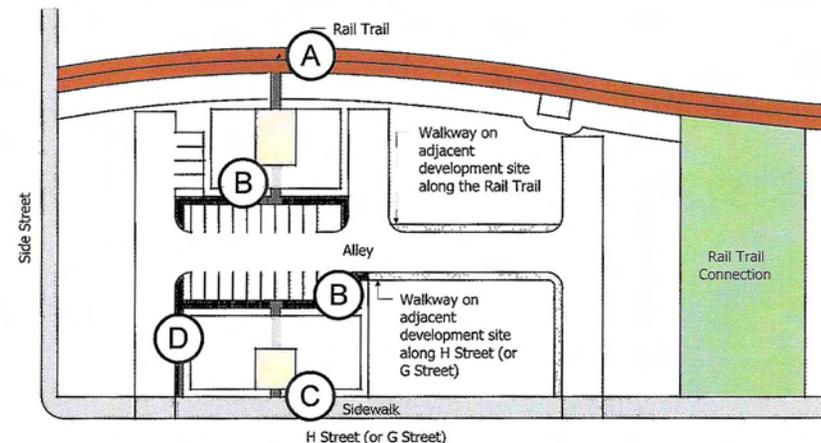
Courtyard Flats are two story buildings designed for stacked residential units (condominiums or apartments). The residential units create a “U” shaped building that surrounds a common courtyard.



Example of courtyard flat building

Pedestrian Connections

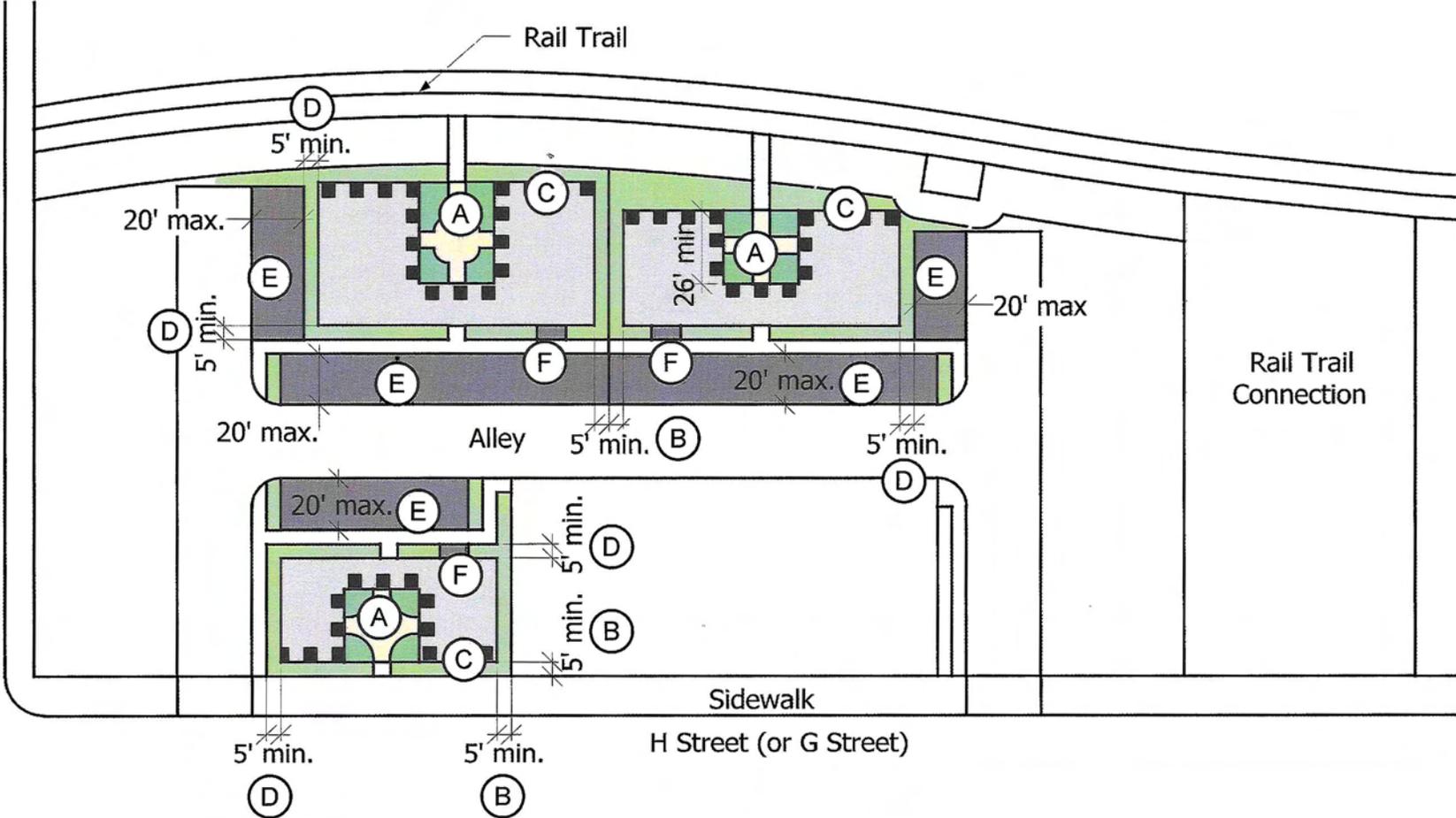
- A. Development sites along the Rail Trail shall contain a walkway connecting the Rail Trail to the courtyard.
- B. All development sites shall contain a walkway connecting the courtyard to the rear corners of the site. This walkway may go through the building. This standard is required to connect all development sites along the Rail Trail and H Street (or G Street).
- C. Development sites along H Street or G Street shall contain a walkway that connects the sidewalk to the courtyard.
- D. Development sites along H Street or G Street shall contain a walkway along the alley that connects the sidewalk to the rear corners of the development site. This standard is required to connect the development sites along the Rail Trail to the sidewalk along H Street or G Street.
- E. All walkways shall be a minimum of 5 feet wide.



Site Design (see Illustration on the following page)

- A. A courtyard is required for each building. Three sides of the courtyard shall be surrounded by the building. No dimensions of courtyards may be less than 26 feet. Courtyards shall open up to the Rail Trail (or G Street or the Santa Rita Extension if the development site is not adjacent to the Rail Trail). Courtyards shall contain a combination of hardscaped and landscaped surfaces. The entrance to all units shall be accessible from the courtyard.
- B. Building Setbacks: Buildings shall be setback at least.
- 5 feet from the front property line along H Street (or G Street). A setback is not required from the front property line along the Rail Trail.
 - 5 feet from side and rear property lines. Building setbacks may need to extend beyond 5 feet to accommodate Standards D and E below.
- C. Build-to-Line (BTL): There is no specific BTL for this building type. However, front facades shall face the Rail Trail (or G Street) and the interior courtyard.
- D. At least 5 feet of landscaping shall be provided between the building and adjacent walkways and/or parking lots.
- E. Parking and Trash/Recycling Storage: Surface parking and trash/recycling storage areas shall be located within zones along alleys. The maximum depth of the zone shall be 20 feet. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the courtyard flat building. Parking and service zones are not allowed along alleys where an adjacent walkway is required (see Standard D under Pedestrian Connections).
- F. Utility meters shall be located within the landscaped area along the rear facade.
- G. Common mailboxes may be located within the courtyard or a common area within the building.

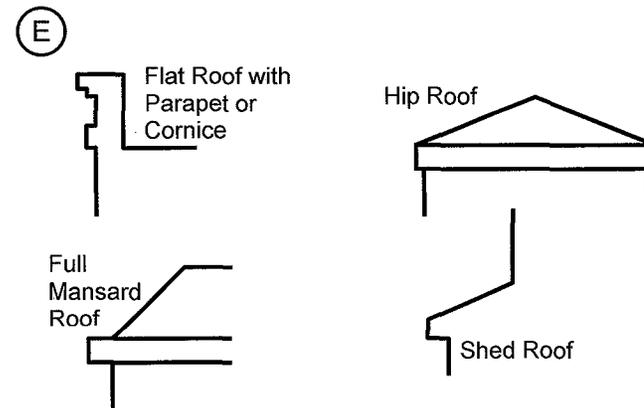
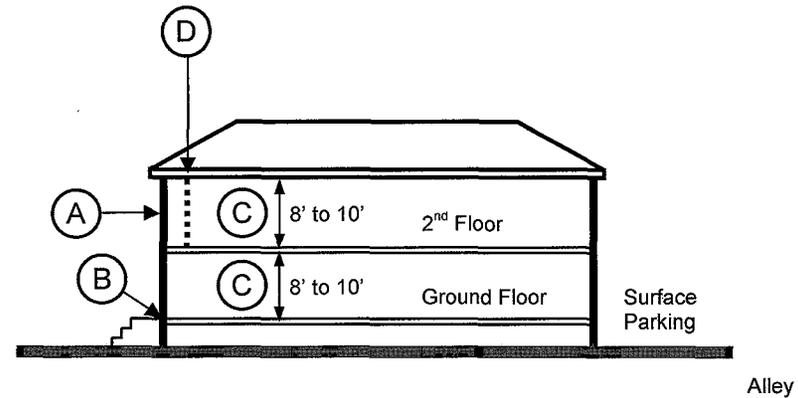
Site Design



■ ■ ■ ■ Front Facade of Building(s)

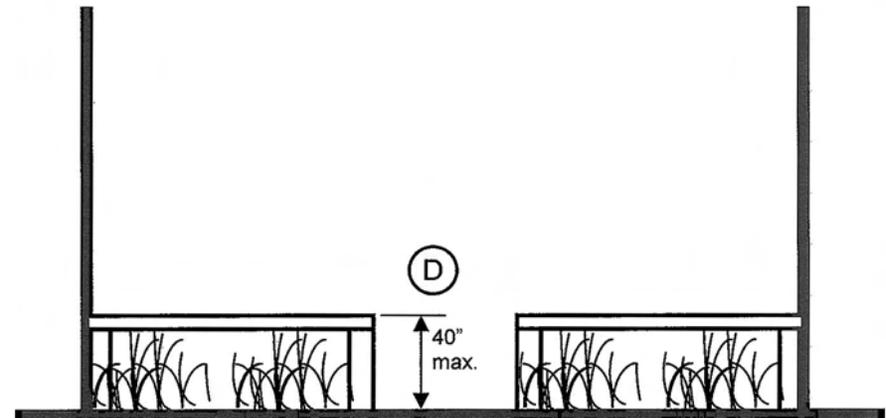
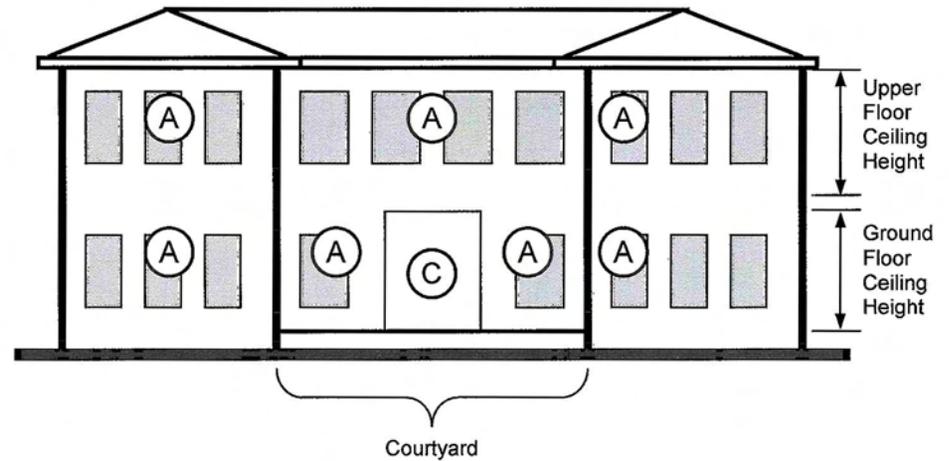
Mass and Height

- A. Buildings shall have two floors.
- B. The elevation of the ground level floor plate shall be built within 24 to 48 inches above the adjacent sidewalk or alley
- C. Ceiling heights shall be 8 to 10 feet tall.
- D. Upper floor step-backs are allowed.
- E. The following roof forms are allowed.
 - Flat Roof with Parapet or Cornice
 - Hip Roof
 - Full Mansard Roof
 - Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 15% of the each floor facade's surface area¹ shall consist of windows²
- B. On rear and side facades, at least 10% of the each floor facade's surface area¹ shall consist of windows²
- C. Entrances to all units shall be provided from a facade that faces the the courtyard.
- D. A fence or wall with an opening or gate is allowed along the edge of the courtyard that is not defined by a building. The fence or wall shall be limited to a height of 40 inches. No other fences or walls are allowed.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways and parking facility entrances) are considered windows.

Projections

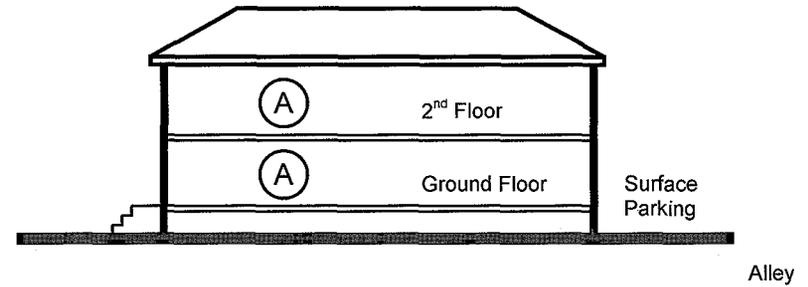
- A. The following building elements may project from ground floor facades and extend into setback areas:
- Stoops
 - Shade Structures: Window Shades and Trellises
- B. The following building elements may project from upper floor facades and extend over setback areas:
- Shade Structures: Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas:
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on all floors of the building:

- Condominium Units
- Apartment Units
- Home occupations in compliance with 9-3.228 of the Zoning Ordinance.

B All other uses are prohibited.



Parking Requirements

- A. One parking space is required for all units that are less than 1,000 square feet in size. Two parking spaces are required for all units that are 1,000 square feet or greater in size. All required parking shall be provided within the development site. Every unit shall have at least one dedicated parking spot. Off-street guest parking spaces are not required.

- B. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per unit. Bike parking shall be located within parking areas.

5.6 In-Line Townhouse Building

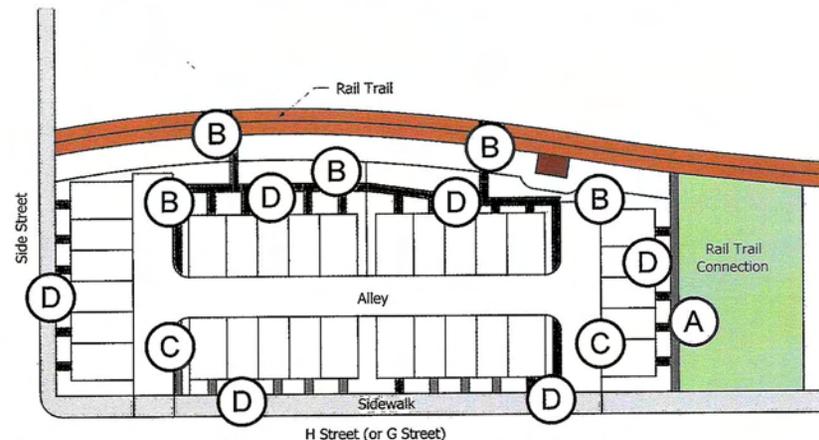
In-Line Townhouses are two story residential units that are placed side-by-side to form a row of houses that face a street or Rail Trail Connection. The garage is accessed from a rear alley.



Examples of in-line townhouse buildings

Pedestrian Connections

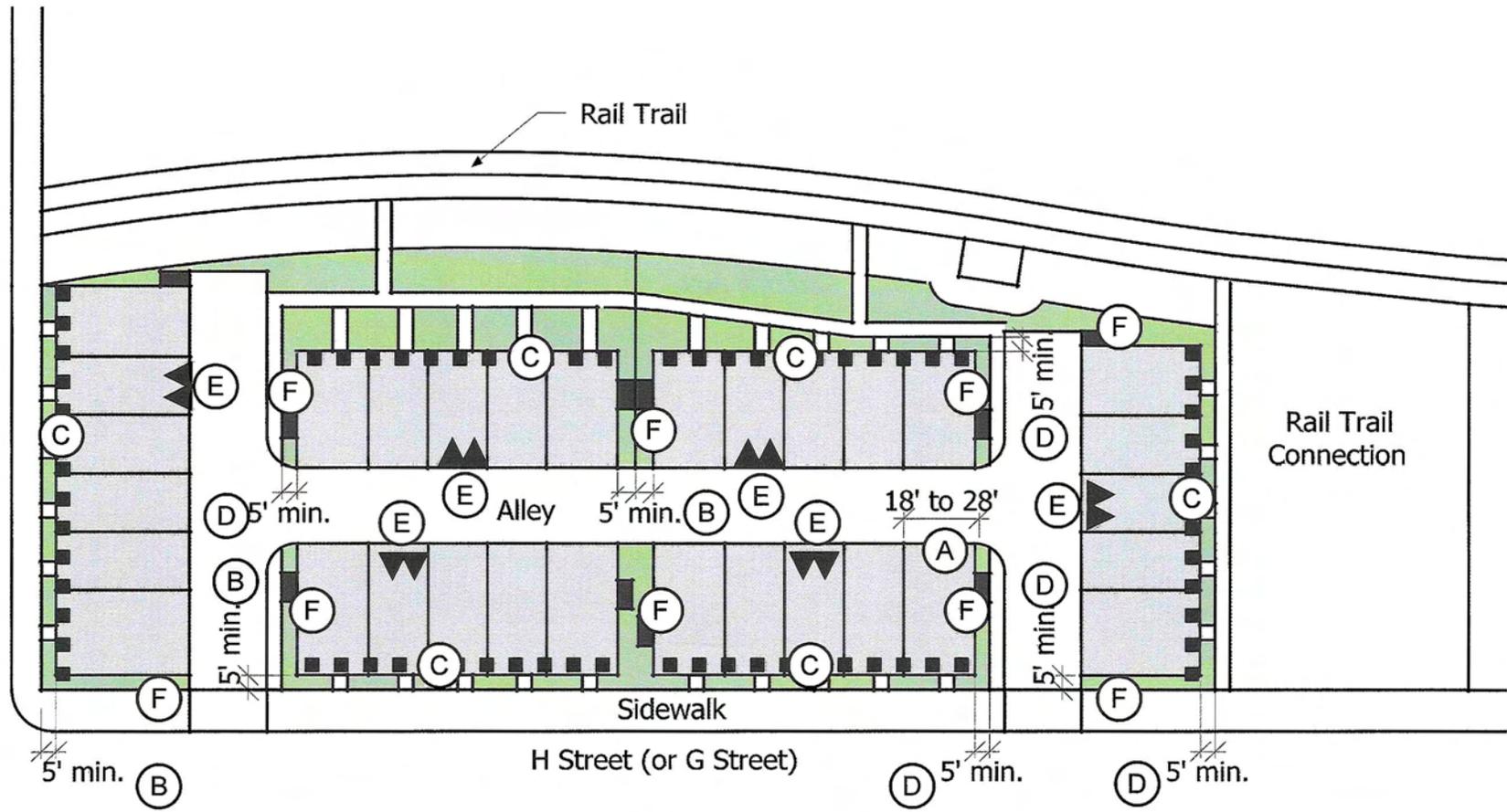
- A. The development site along the Rail Trail Connection shall contain a common walkway that connects the Rail Trail to the sidewalk on H Street (or G Street). This walkway and the adjacent edge of the Rail Trail Connection shall function as an emergency access area (see additional standards in Section 8.1)
- B. All development sites along the Rail Trail shall contain a common walkway along the front of the units that connects the rear corners of the development site to the Rail Trail and the adjacent development site.
- C. The development sites along H Street (or G Street) shall contain walkways along the alleys to connect the sidewalk on H Street (or G Street) to the development sites along the Rail Trail.
- D. Each townhouse shall have a private walkway connecting the entrance of the unit to either a sidewalk or common walkway.
- E. All walkways shall be a minimum of 5 feet wide.



Site Design (see Illustration on the following page)

- A. Lot Width: Townhouse lots shall have a width that ranges from 18 to 28 feet.
- B. Building Setbacks Buildings shall be setback at least
 - 5 feet from the front and external side property lines (side property lines adjacent to a street or alley) Building setbacks may need to extend beyond 5 feet to accommodate Standard D below.
 - No setbacks are required from the rear property line and from internal side property lines (property lines between two adjacent townhouse units).
- C. Build-to-Line (BTL): The BTL shall be located within 5 to 10 feet of front property lines that are adjacent to a street or the Rail Trail Connection. The front facade of the units shall be built along the BTL. There is no specific BTL for the development site along the Rail Trail. However the front facade of the building shall face the Rail Trail.
- D. At least 5 feet of landscaping shall be provided between buildings and common walkways on the development site.
- E. Parking and Trash/Recycling Storage: Each townhouse shall have a two-car garage that is accessible from the alley. Trash, recycling, and green waste bins shall be stored in the garage of each unit.
- F. Utility meters and common mailboxes shall be located within the landscaped area along the side facade of end units. As an alternative, the utility meters for each unit may be located along the rear facade within an equipment enclosure that opens to the alley.

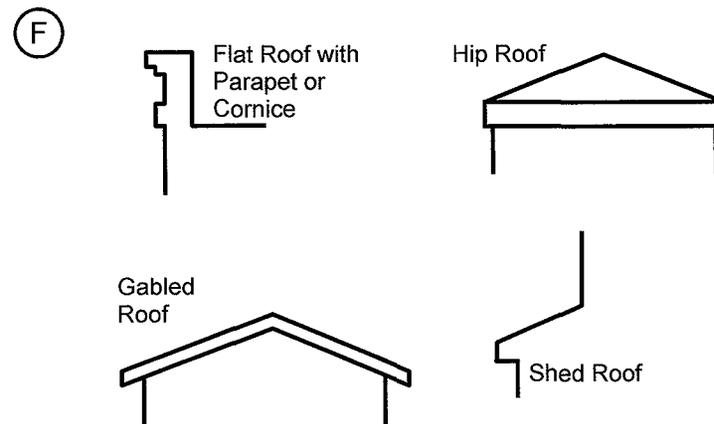
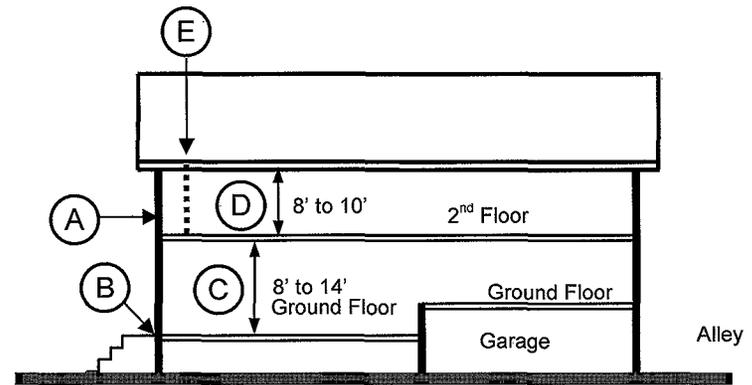
Site Design



■ ■ ■ ■ Front Facade of Building(s)

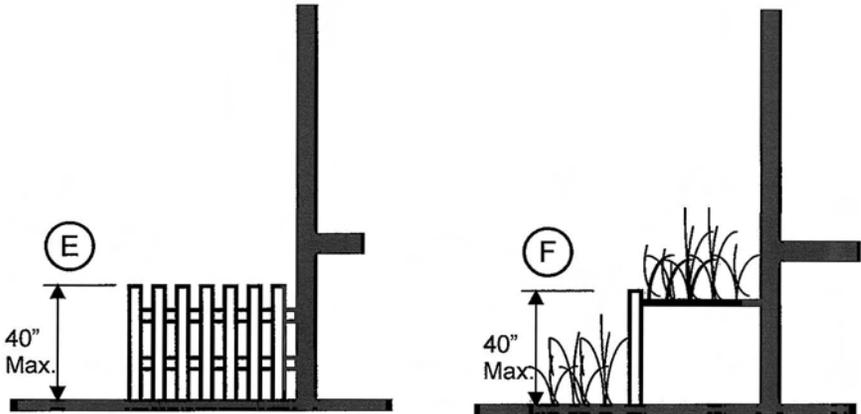
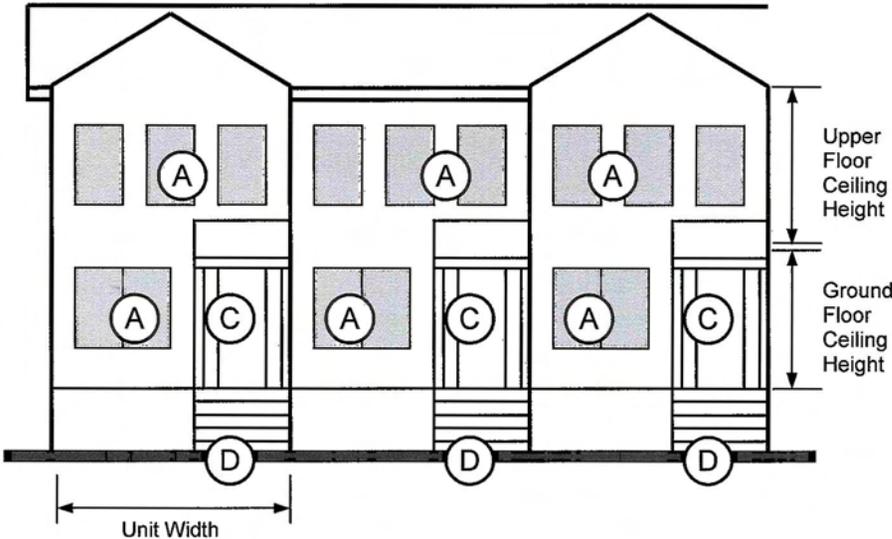
Mass and Height

- A. Each unit shall have two floors. A parking level is allowed beneath each unit and shall not be counted as a floor.
- B. The elevation of the ground level floor plate along the front facade shall be built 36 to 48 inches above the elevation of the adjacent sidewalk or common walkway.
- C. Ground floor ceiling heights shall be 8 to 14 feet tall.
- D. Upper floor and ground floor ceiling heights shall be 8 to 10 feet tall.
- E. Upper floor step-backs are allowed.
- F. The following roof forms are allowed:
 - Flat Roof with Parapet or Cornice
 - Hip Roof
 - Gabled Roof
 - Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 15% of the each floor facade’s surface area¹ shall consist of windows²
- B. On rear facades and side facades adjacent to an alley or street, at least 10% of the each floor facade’s surface area¹ shall consist of windows² This standard does not apply to the rear parking level facade
- C. Each unit shall have a private entrance on the front facade.
- D. Each unit shall have a stoop or front porch that provides access to the front entrance. All stoops and front porches shall be designed in compliance with the stoop standards in Section 6.14 (Front Porches and Stoops) Stoops may be recessed into the facade or may project out from the facade
- E. Fences are only allowed within front yards. Fences shall be limited to a height of 40 inches and shall have a minimum transparency of 25%. No other fences are allowed
- F. Retaining walls are allowed to create a terraced planter along the front facade. Retaining wall shall be limited to a height of 40 inches. Landscaping shall be placed in front of the retaining wall.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building
² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways) are considered windows.

Projections

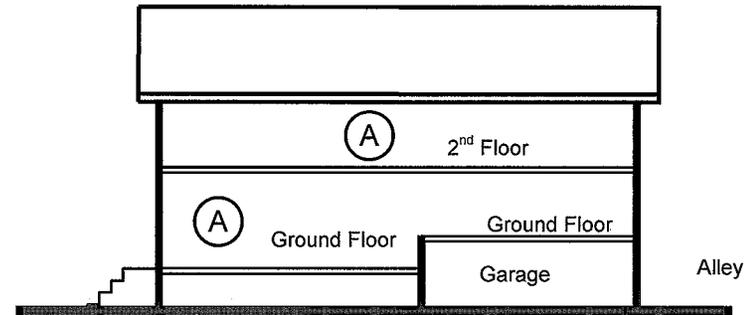
- A. The following building elements may project from ground floor facades and extend into setback areas.
- Front porches and stoops
 - Shade Structures: Window Shades and Trellises
- B. The following building elements may project from upper floor facades and extend over setback areas:
- Shade Structures Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas:
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- E. Standards for the above projections are provided in Section 6 0 (Architectural Design Standards).

Building Uses

A. The following uses are allowed on all floors of the building.

- Condominium Units
- Apartment Units
- Home occupations in compliance with 9-3 228 of the Zoning Ordinance.

B All other uses are prohibited.



Parking Requirements

- A All units shall have a two car parking garage. Side-by-side and tandem parking arrangements are allowed within the garage.
- B No off-street guest parking is required.
- C Each garage shall be designed with enough room to park at least one bicycle.

5.7 Cluster Townhouse Building

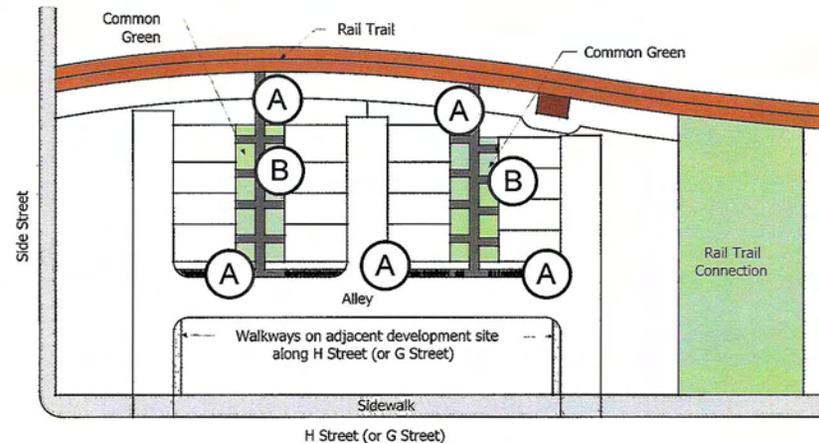
Cluster Townhouses are two story residential units that are placed side-by-side to form two rows of houses that face each other and are separated by a common green. The garage is accessed from a rear alley



Example of cluster townhouse buildings

Pedestrian Connections

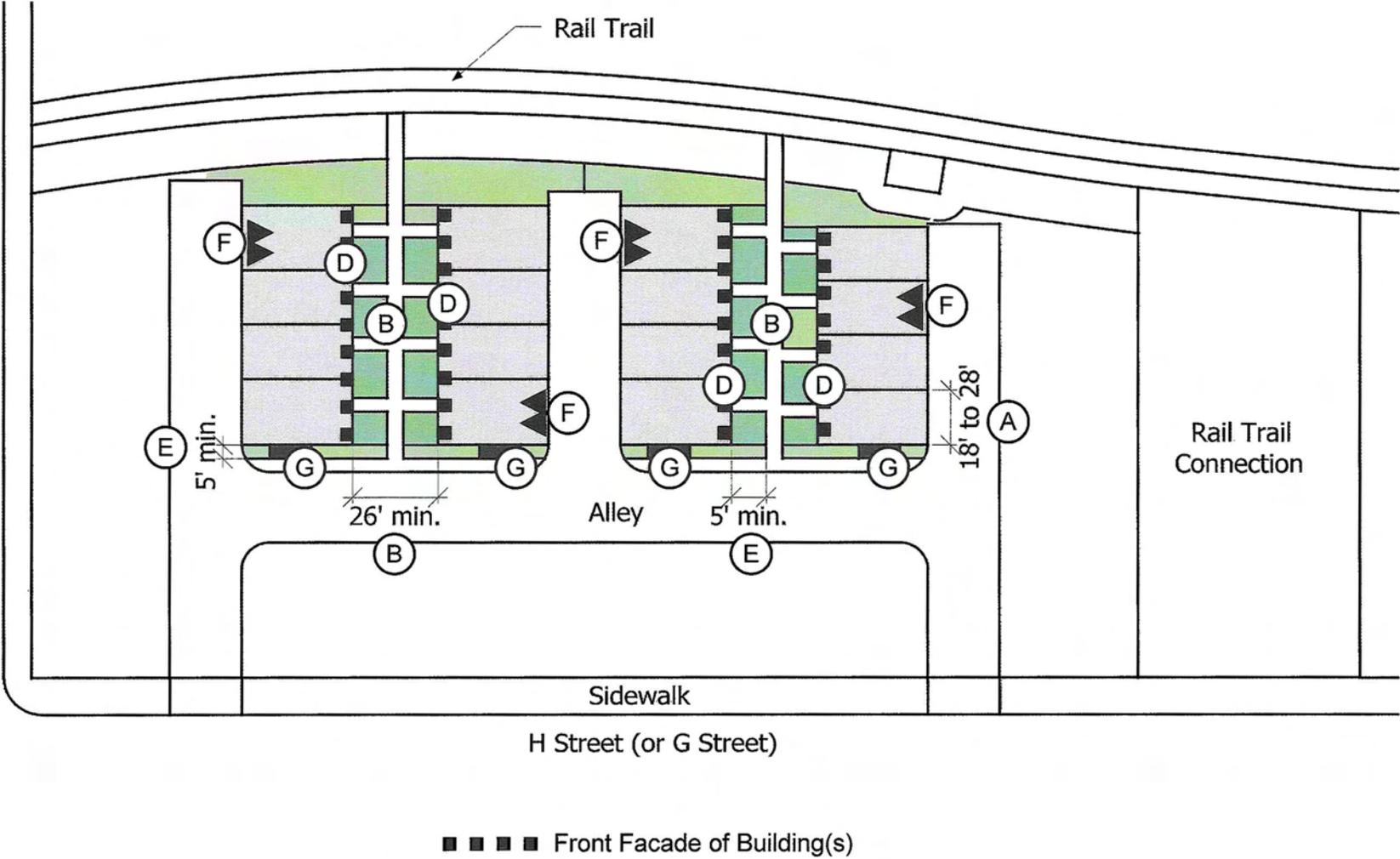
- A. Development sites shall contain a common walkway that connects the rear corners of the development site to the Rail Trail. This common walkway will go through the common green. This standard is required to ensure that adjacent development sites are connected
- B. Each townhouse shall have a private walkway connecting the entrance of the unit to the common walkway
- C. All walkways shall be a minimum of 5 feet wide.



Site Design (see Illustration on the following page)

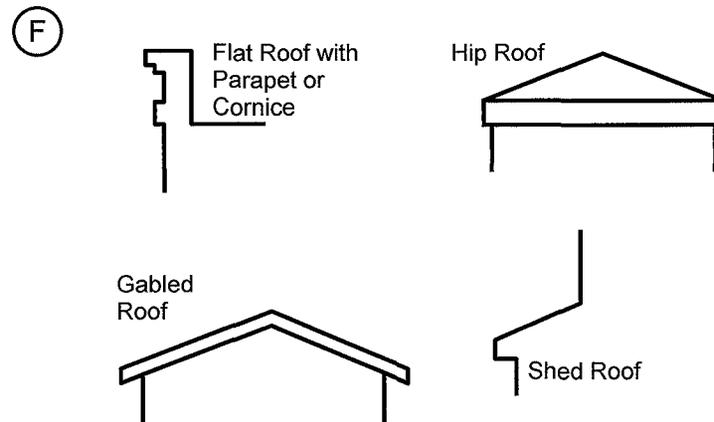
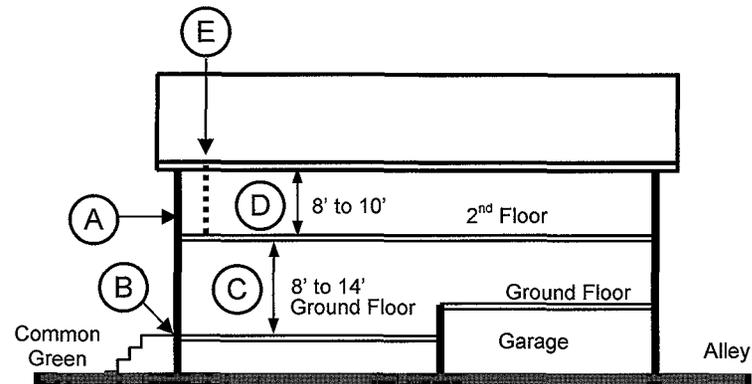
- A. Lot Width: Townhouse lots shall have a width that ranges from 18 to 28 feet.
- B. Common Green: Each development site shall contain a common green that is at least 26 feet wide. Townhouse lots shall be located on opposite sides of the common green.
- C. Building Setbacks: There are no specific building setbacks for this building type. However, building setbacks may be required to accommodate Standard E below.
- D. Build-to-Line (BTL): The BTL is located along the edges of the common green. The front facade of the units shall be built along the BTL.
- E. At least 5 feet of landscaping shall be provided between buildings and common walkways on the development site.
- F. Parking and Trash/Recycling Storage: Each townhouse shall have a two-car garage that is accessible from the alley. Trash, recycling, and green waste bins shall be stored in the garage of each unit.
- G. Utility meters and common mailboxes shall be located within the landscaped area along the end units that are adjacent to the alley intersections. As an alternative, the utility meters for each unit may be located within the garage within an equipment enclosure that opens to the alley.

Site Design



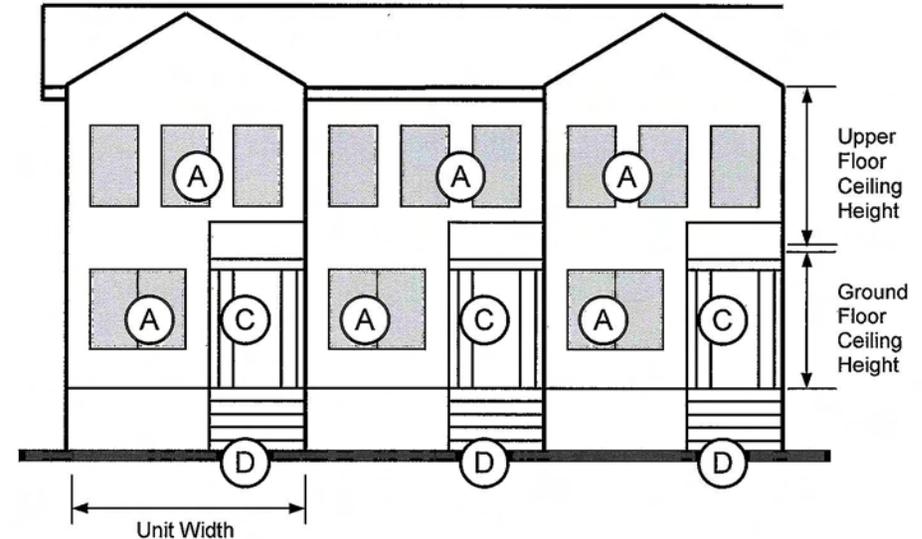
Mass and Height

- A. Each unit shall have two floors. A parking level is allowed beneath each unit and shall not be counted as a floor
- B. The elevation of the ground level floor plate along the front facade shall be built 36 to 48 inches above the elevation of the common green.
- C. Ground floor ceiling heights shall be 8 to 14 feet tall
- D. Upper floor and ground floor ceiling heights shall be 8 to 10 feet tall.
- E. Upper floor step-backs are allowed.
- F. The following roof forms are allowed:
 - Flat Roof with Parapet or Cornice
 - Hip Roof
 - Gabled Roof
 - Shed Roof (only allowed where step-backs occur)



Window Fenestration and Frontages

- A. On front facades, at least 15% of the each floor facade's surface area¹ shall consist of windows²
- B. On rear facades and side facades adjacent to an alley or the Rail Trail, at least 10% of the each floor facade's surface area¹ shall consist of windows². This standard does not apply to the rear parking level facade.
- C. Each unit shall have a private entrance on the front facade.
- D. Each unit shall have a stoop or front porch that provides access to the front entrance. All stoops and front porches shall be designed in compliance with the stoop standards in Section 6.14 (Front Porches and Stoops). Stoops may be recessed into the facade or may project out from the facade.
- E. Fences and walls are prohibited with this building type.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways) are considered windows.

Projections

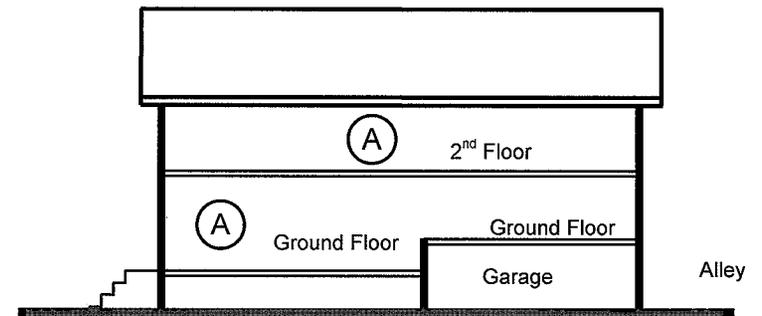
- A. The following building elements may project from ground floor facades and extend into setback areas and the common green
- Front porches and stoops
 - Shade Structures: Window Shades and Trellises
- B. The following building elements may project from upper floor facades and extend over setback areas and the common green:
- Shade Structures Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas or the common green:
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards)

Building Uses

A. The following uses are allowed on all floors of the building.

- Condominium Units
- Apartment Units
- Home occupations in compliance with 9-3.228 of the Zoning Ordinance

B. All other uses are prohibited.



Parking Requirements

- A. All units shall have a two car parking garage. Side-by-side and tandem parking arrangements are allowed within the garage.
- B. No off-street guest parking is required
- C. Each garage shall be designed with enough room to park at least one bicycle.

5.8 Rear Yard Townhouse Building

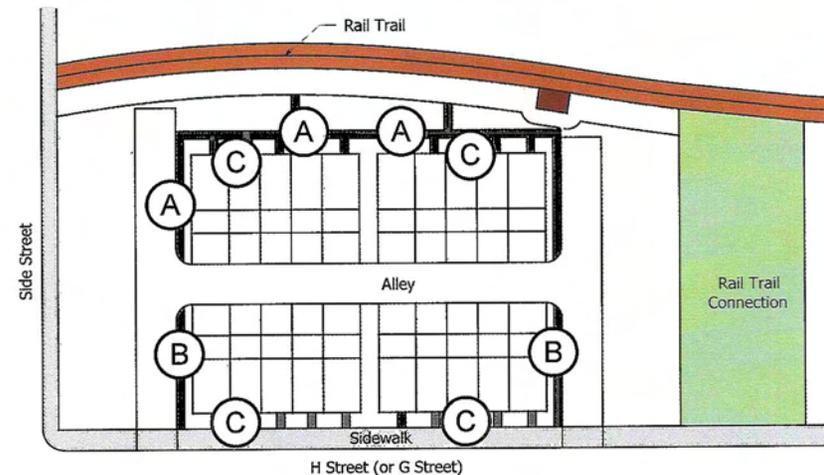
Rear Yard Townhouses are two story residential units that are placed side-by-side to form a row of houses that face a street. Each unit contains a yard that is located between the unit and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.



Example of rear yard townhouse buildings

Pedestrian Connections

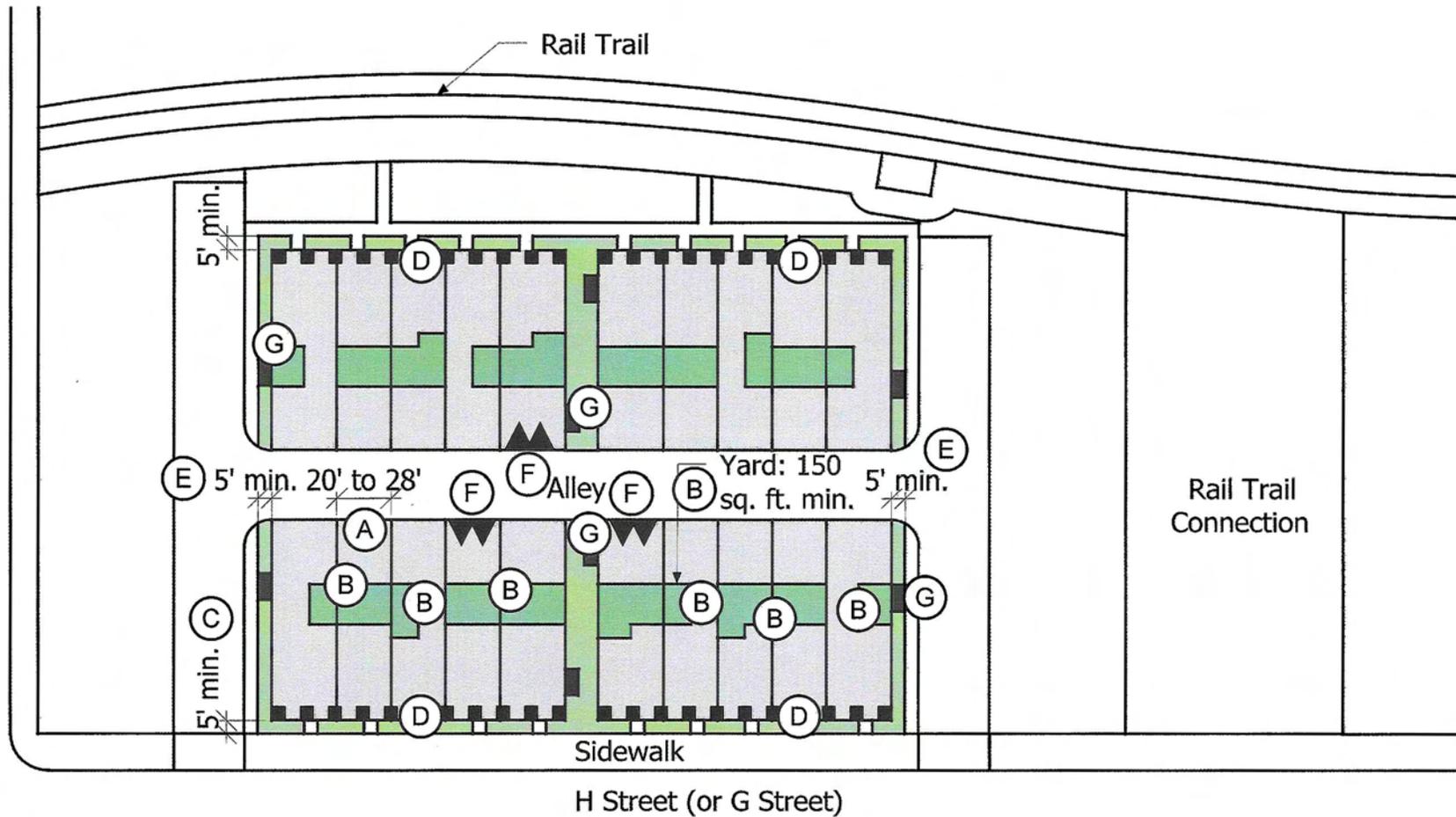
- D. All development sites along the Rail Trail shall contain a common walkway along the front of the units that connects the rear corners of the development site to the Rail Trail and the adjacent development site.
- E. The development sites along H Street (or G Street) shall contain walkways along the alleys to connect the sidewalk on H Street (or G Street) to the development sites along the Rail Trail.
- F. Each townhouse shall have a private walkway connecting the entrance of the unit to either a sidewalk or common walkway.
- G. All walkways shall be a minimum of 5 feet wide.



Site Design (see Illustration on the following page)

- A. Lot Width: Townhouse lots shall have a width that ranges from 20 to 28 feet
- B. Rear Yards: Each townhouse lot shall contain a rear yard/side yard that is at least 150 square feet in size with no dimension less than 10 feet. The yard shall be located within between the townhouse and the attached/detached garage.
- C. Building Setbacks: Buildings shall be setback at least.
 - 5 feet from the front and external side property lines (side property lines adjacent to a street or alley). Building setbacks may need to extend beyond 5 feet to accommodate Standard E below.
 - No setbacks are required from the rear property line and from internal side property lines (property lines between two adjacent townhouse units).
- D. Build-to-Line (BTL). The BTL is located within 5 to 10 feet of front property lines. The front facade of the units shall be built along the BTL. There is no specific BTL for the development site along the Rail Trail. However the front facade of the building shall face the Rail Trail.
- E. At least 5 feet of landscaping shall be provided between buildings and common walkways on the development site
- F. Parking and Trash/Recycling Storage: Each townhouse shall have an attached or detached two-car garage that is accessible from the alley. Trash, recycling, and green waste bins shall be stored in the garage of each unit.
- G. Utility meters and common mailboxes shall be located within the landscaped area along the end units that are adjacent to the alley intersections. As an alternative, the utility meters for each unit may be located within the garage within an equipment enclosure that opens to the alley.

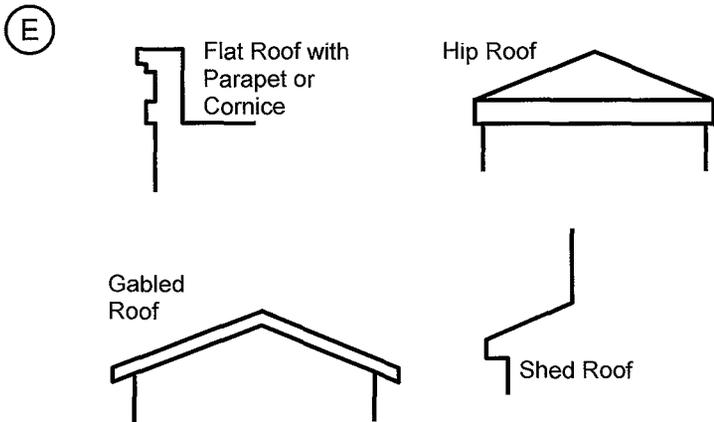
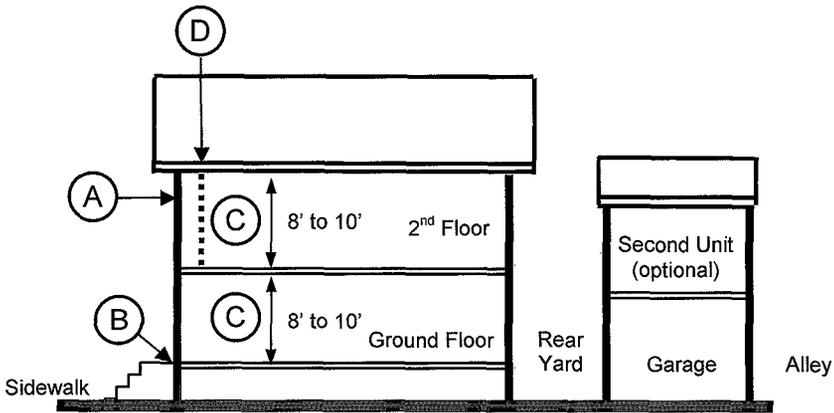
Site Design



■ ■ ■ ■ Front Facade of Building(s)

Mass and Height

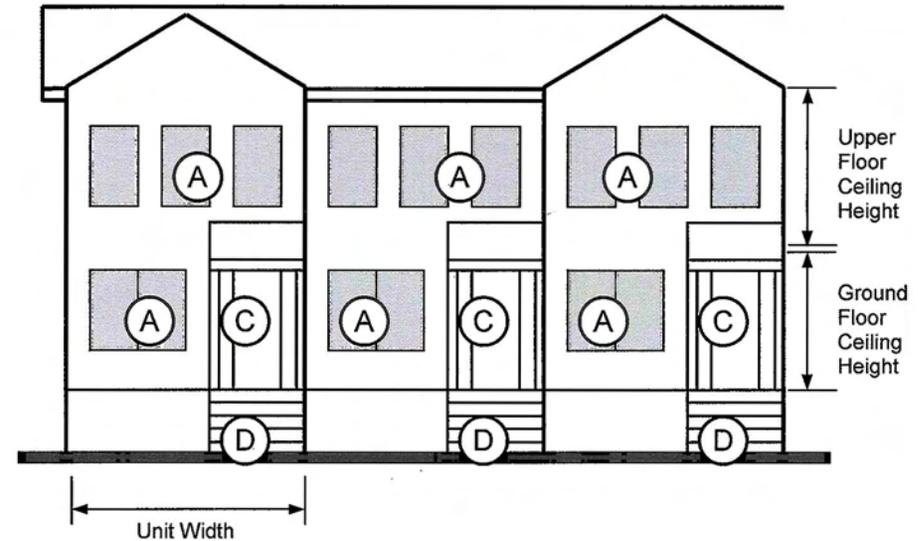
- A. Buildings shall have two floors.
- B. The elevation of the ground level floor plate along the front facade shall be built 36 inches to 48 inches above the elevation of the adjacent sidewalk
- C. Ceiling heights shall be 8 to 10 feet tall
- D. Upper floor step-backs are allowed.
- E. The following roof forms are allowed.
 - Flat Roof with Parapet or Cornice
 - Hip Roof
 - Gabled Roof
 - Shed Roof (only allowed where step-backs occur)



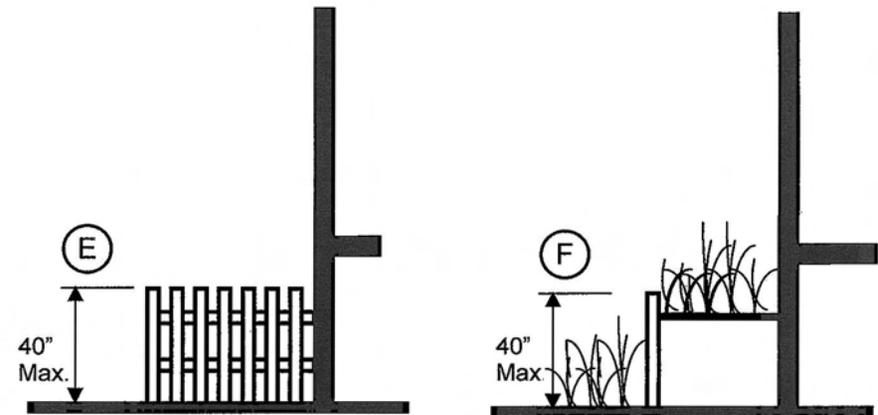


Window Fenestration and Frontages

- A. On front facades, at least 15% of the each floor facade's surface area¹ shall consist of windows²
- B. On rear and side facades, at least 10% of the each floor facade's surface area¹ shall consist of windows². This standard does not apply to the rear parking level facade
- C. Each unit shall have a private entrance on the front facade
- D. Each unit shall have a stoop or front porch that provides access to the front entrance. All stoops and front porches shall be designed in compliance with the stoop standards in Section 6.14 (Front Porches and Stoops). Stoops may be recessed into the facade or may project out from the facade.
- E. Front yard fences shall be limited to a height of 40 inches and shall have a minimum transparency of 25%. No other fences are allowed.
- F. Retaining walls are allowed to create a terraced planter along the front facade. Retaining wall shall be limited to a height of 40 inches. Landscaping shall be placed in front of the retaining wall.
- G. Fences and walls that are used to enclose the rear or side yard shall have a maximum height of six feet.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.
² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways) are considered windows.



Projections

- A. The following building elements may project from ground floor facades and extend into setback areas:
- Front porches and stoops
 - Shade Structures: Window Shades and Trellises
- B. The following building elements may project from upper floor facades and extend over setback areas:
- Shade Structures: Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas.
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards)

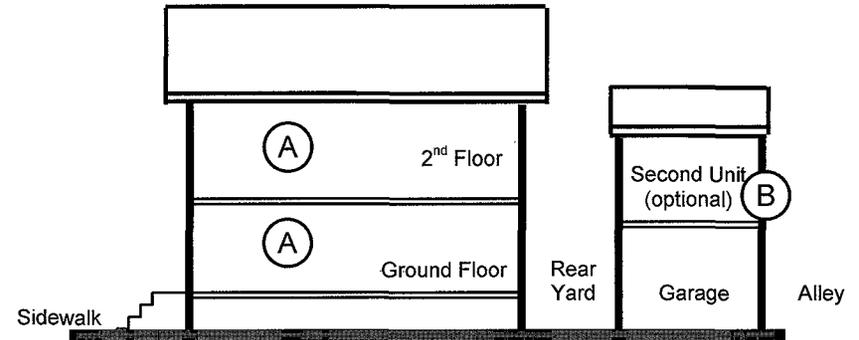
Building Uses

A. The following uses are allowed on all floors of the building:

- Condominium Units
- Apartment Units
- Home occupations in compliance with 9-3.228 of the Zoning Ordinance.

B. A second unit, as defined in Section 10.0, is allowed above the garage.

C. All other uses are prohibited.



Parking Requirements

- A. All units shall have a two car parking garage. Side-by-side and tandem parking arrangements are allowed within the garage.
- B. If a second unit is provided, one additional parking space is required. This space may be located within the garage or an uncovered parking space that is accessible from the rear alley
- C. No off-street guest parking is required
- D. Each garage shall be designed with enough room to park at least one bicycle.

5.9 Cottage House Buildings

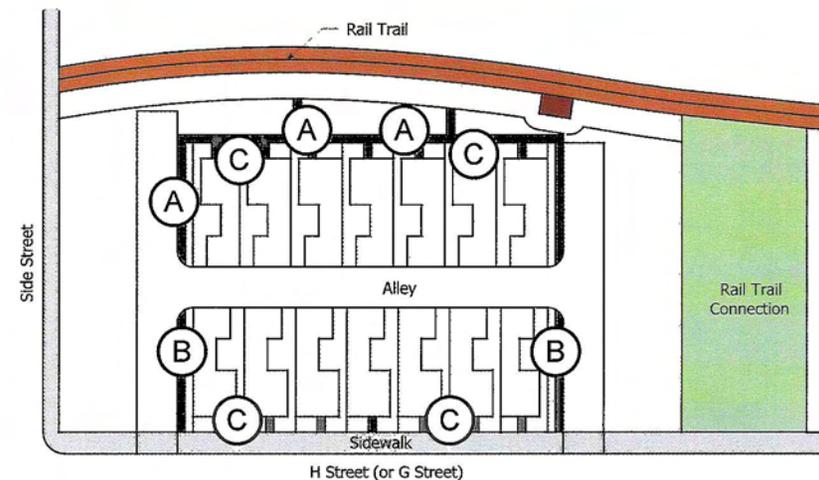
Cottage houses are one to two story houses that have yards and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.



Examples of cottage house buildings

Pedestrian Connections

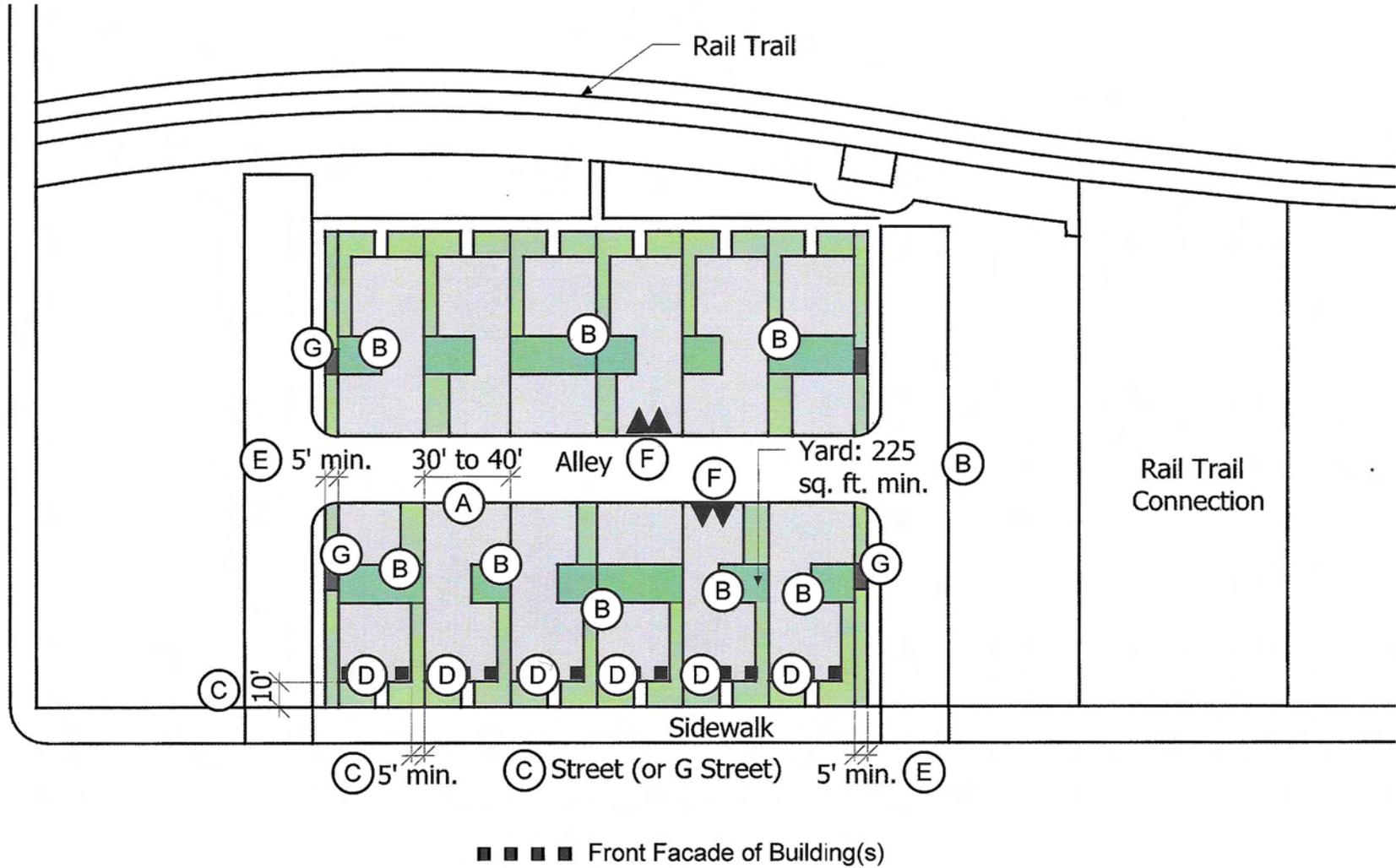
- A. All development sites along the Rail Trail shall contain a common walkway along the front of the units that connects the rear corners of the development site to the Rail Trail and the adjacent development site
- B. The development sites along H Street (or G Street) shall contain walkways along the alleys to connect the sidewalk on H Street (or G Street) to the development sites along the Rail Trail.
- C. Each cottage house shall have a private walkway connecting the entrance of the unit to either a sidewalk or common walkway
- D. All walkways shall be a minimum of 5 feet wide



Site Design (see Illustration on the following page)

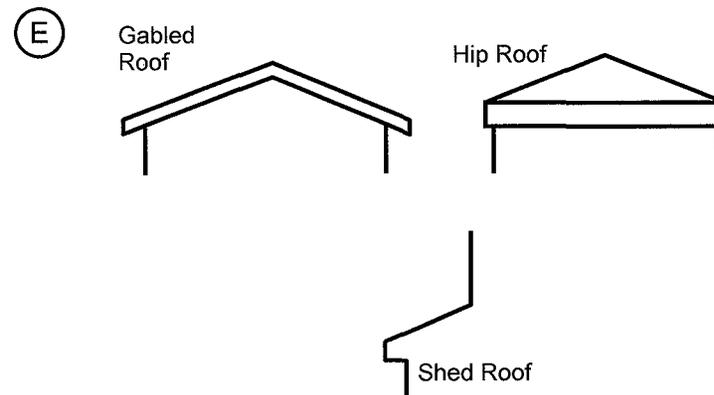
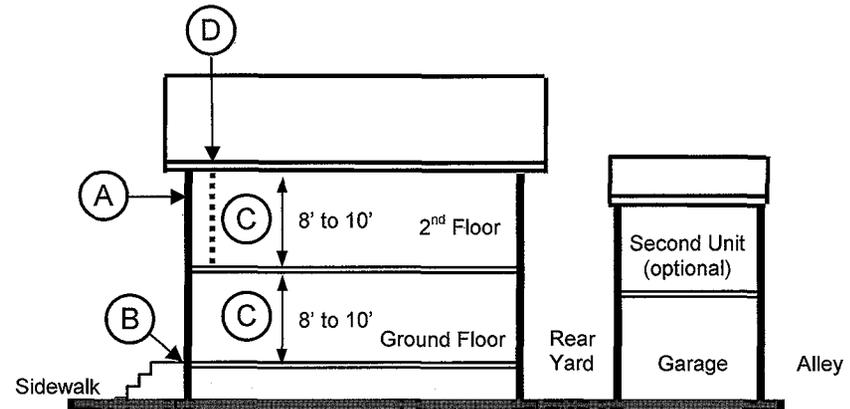
- A. Lot Width: Lots shall have a width that ranges from 30 to 40 feet.
- B. Rear Yards: Each house shall contain a rear yard/side yard that is at least 225 square feet in size with no dimension less than 15 feet.
- C Building Setbacks: Buildings shall be setback at least:
 - 10 feet from the front property lines
 - 5 feet from one of the side property lines A setback of zero is allowed on the other side property line Building setbacks may need to extend beyond 5 feet or zero feet to accommodate Standard E below.
 - No setbacks are required from the rear property line.
- D Build-to-Line (BTL). The BTL is located within 10 to 12 feet of front property lines. The front facade of the units shall be built along the BTL. There is no specific BTL for the development site along the Rail Trail. However the front facade of the building shall face the Rail Trail.
- E At least 5 feet of landscaping shall be provided between buildings and common walkways on the development site.
- F Parking and Trash/Recycling Storage Each townhouse shall have an attached or detached two-car garage that is accessible from the alley. Trash, recycling, and green waste bins shall be stored in the garage of each unit.
- G. Utility meters and common mailboxes shall be located within the landscaped area along the end units that are adjacent to the alley. As an alternative, the utility meters for each unit may be located in the garage within an equipment enclosure that opens to the alley.

Site Design



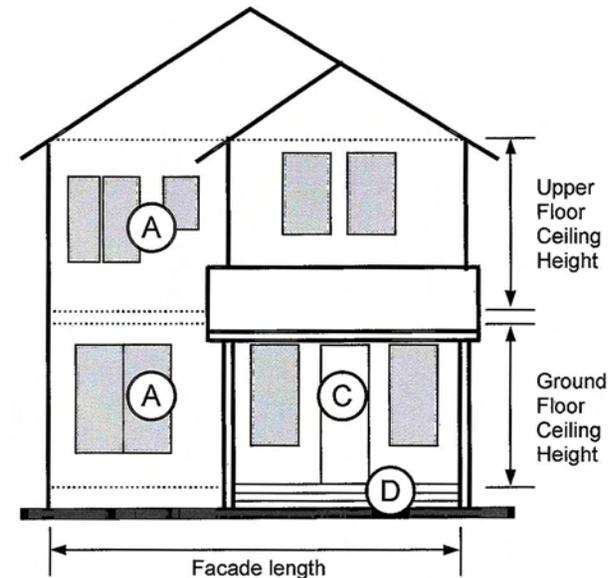
Mass and Height

- A. Buildings shall have two floors.
- B. The elevation of the ground level floor plate along the front facade shall be built 36 inches to 48 inches above the elevation of the adjacent sidewalk.
- C. Ceiling heights shall be 8 to 10 feet tall.
- D. Upper floor step-backs are allowed
- E. The following roof forms are allowed:
 - Hip Roof
 - Gabled Roof
 - Shed Roof (only allowed where step-backs occur)



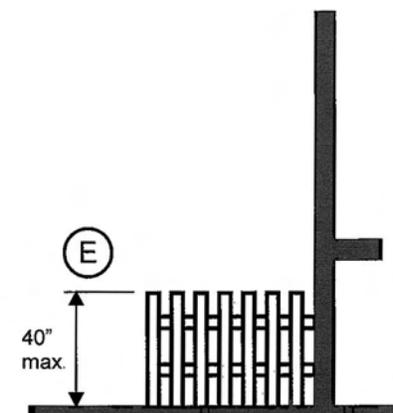
Window Fenestration and Frontages

- A. On front facades, at least 15% of the each floor facade's surface area¹ shall consist of windows²
- B. On rear facades and side facades adjacent to an alley, at least 10% of the each floor facade's surface area¹ shall consist of windows². This standard does not apply to the rear parking level facade
- C. Each unit shall have a private entrance on the front facade.
- D. Each unit shall have a stoop or front porch that provides access to the front entrance. All stoops and front porches shall be designed in compliance with the stoop standards in Section 6.14 (Front Porches and Stoops). Stoops may be recessed into the facade or may project out from the facade
- E. Front yard fences shall be limited to a height of 40 inches and shall have a minimum transparency of 25%. No other fences are allowed.
- F. Fences and walls that are used to enclose the rear or side yard shall have a maximum height of 6 feet.



¹ As measured by multiplying the facade length by the floor-to-floor height of each floor of the building.

² All parts of the window (such as head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing shall be included in the calculation. Windows on doors and unglazed openings in the facades (such as covered walkways) are considered windows.

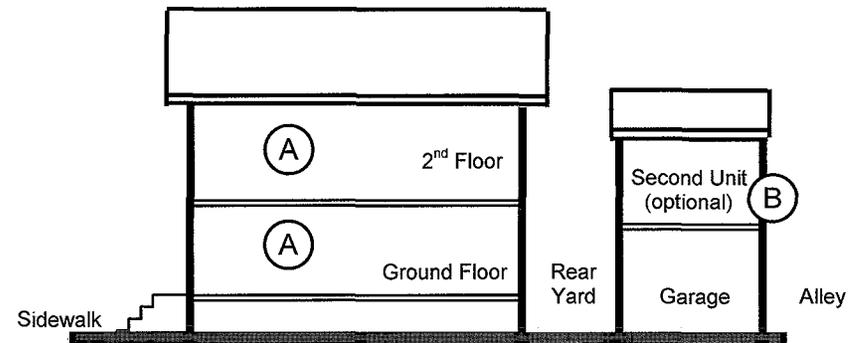


Projections

- A. The following building elements may project from ground floor facades and extend into setback areas.
- Front porches and stoops
 - Shade Structures: Window Shades and Trellises
- B. The following building elements may project from upper floor facades and extend over setback areas:
- Shade Structures: Window Shades and Trellises
- C. The following building elements may project from facades, but are not allowed to extend into setback areas.
- Balconies
 - Bay Windows
- D. Roof projections and overhangs may project up to 3 feet from facades and may encroach over an adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- E. Standards for the above projections are provided in Section 6.0 (Architectural Design Standards).

Building Uses

- A. The following uses are allowed on all floors of the building:
 - Single-family residential
 - Home occupations in compliance with 9-3 228 of the Zoning Ordinance
- B. A second unit, as defined in Section 10.0, is allowed above the garage.
- C. All other uses are prohibited.



Parking Requirements

- A. All houses shall have a two car parking garage. Side-by-side and tandem parking arrangements are allowed within the garage.
- B. If a second unit is provided, one additional parking space is required. This space may be located within the garage or an uncovered parking space that is accessible from the rear alley.
- C. No off-street guest parking is required.
- D. Each garage shall be designed with enough room to park at least one bicycle.

5.10 Family Entertainment Center Buildings

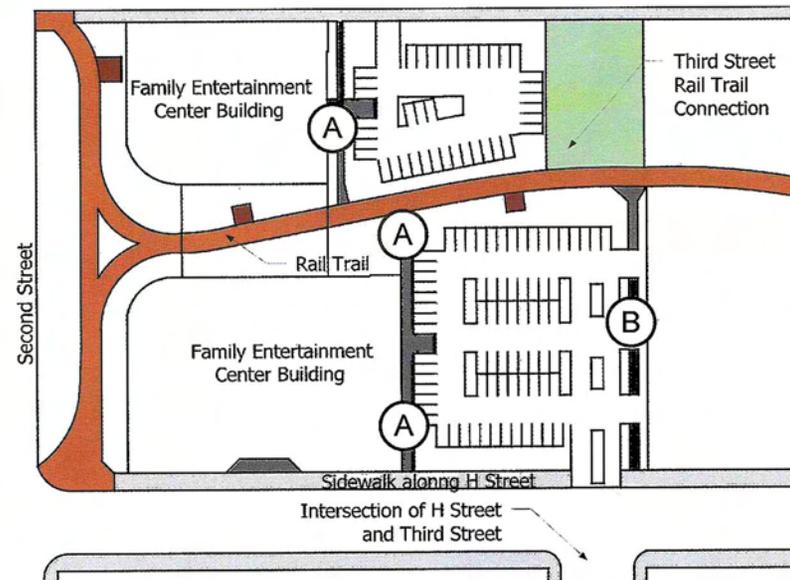
Family Entertainment Center Buildings are one or two story buildings designed for a variety of family entertainment uses.



Example of a family entertainment center building

Pedestrian Connections

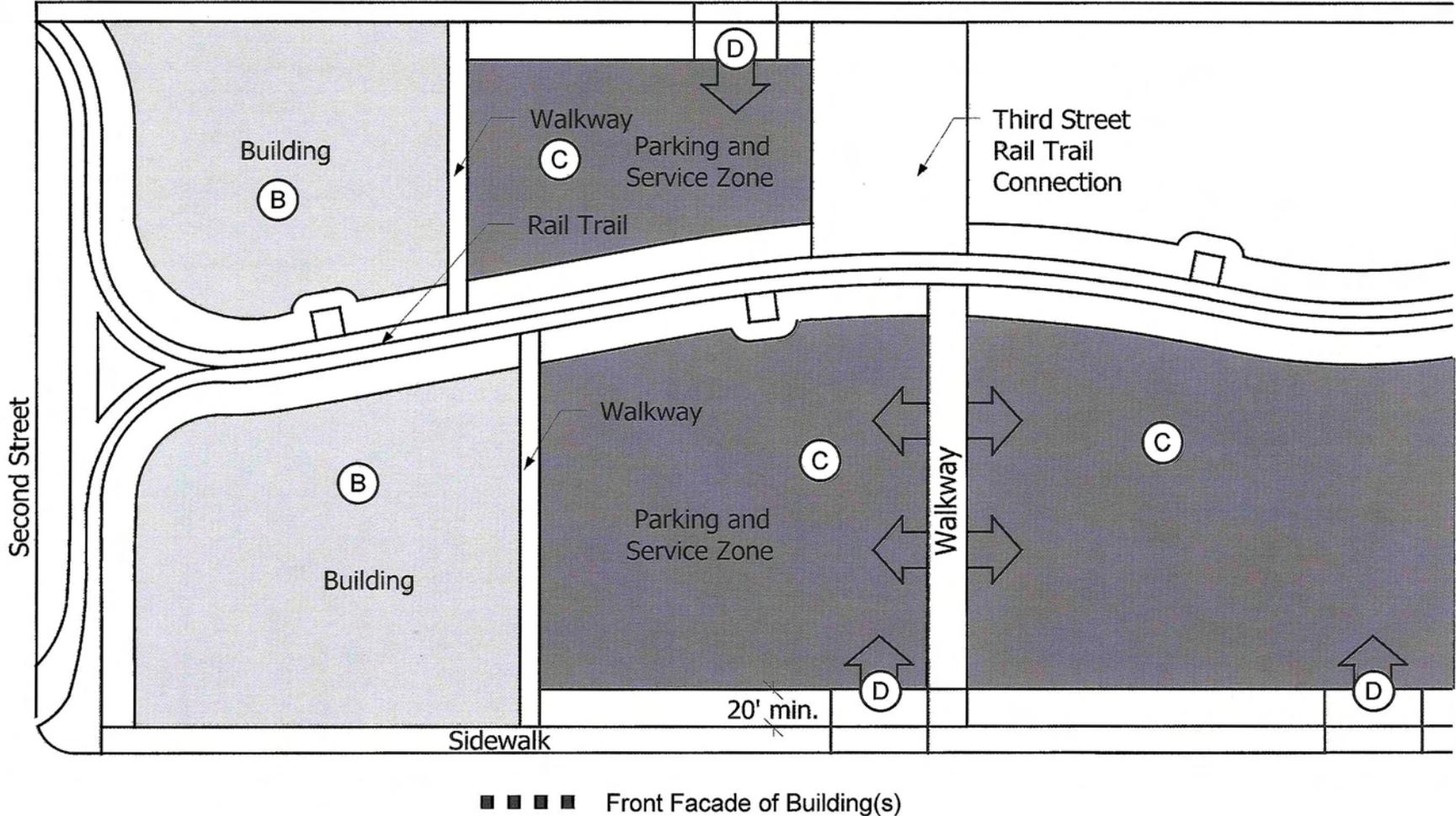
- A. A walkway shall be provided along the eastern edges of the Family Entertainment Center Buildings to connect H Street and G Street to the Rail Trail.
- B. A walkway that connects the H Street and Third Street intersection to the Rail Trail and the Third Street Rail Trail Connection north of the Rail Trail shall be provided. This walkway shall include landscaped parkways on each side. These parkways shall each be a minimum of 5 feet wide.
- C. All walkways shall be a minimum of 6 feet wide



Site Design (see Illustration on the following page)

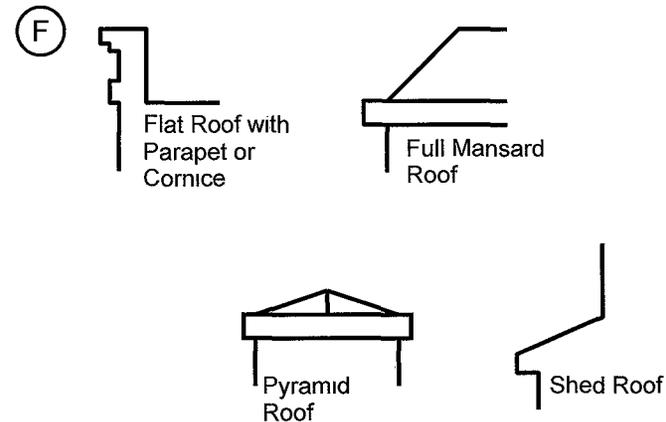
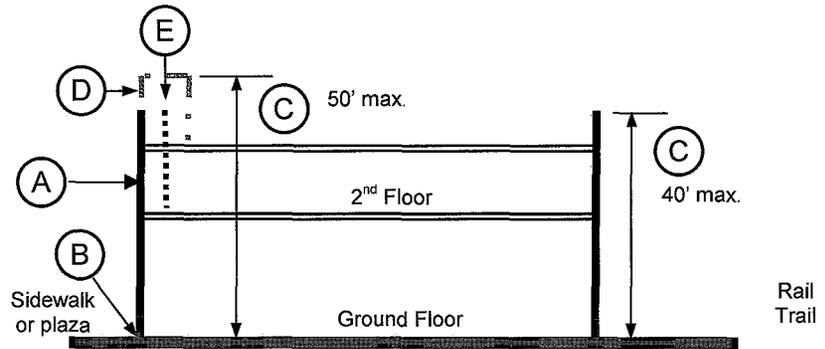
- A. Building Setbacks: No setbacks are required.
- B. Build-to-Line (BTL): There is no specific BTL for this building type. However, the Family Entertainment Center Buildings shall be located along the western edges of the block along the West Entry and Second Street.
- C. Parking and Service Zone: This zone includes the portions of the development site that are to the east of the Family Entertainment Center Buildings and that are setback at least 20 feet from H Street and G Street. If provided surface parking, trash/recycling storage areas, delivery zones, and service areas shall be located in this zone. However, as an alternative, delivery zones and service areas may be located along a street if the delivery/service entrance is designed to look like a primary building entrance. All trash/recycling storage areas shall be sized to accommodate trash, recycling, and green waste bins. All external trash/recycling bins shall be located within a trash enclosure. Trash enclosures shall be designed with materials, colors, and details that are compatible to those used on the commercial buildings.
- D. Access to parking areas shall be provided from H Street or G Street. The primary access driveway to the parking lot to the south of the Rail Trail shall be located at the intersection of H Street and Third Street.
- E. Utility meters shall be located within equipment enclosures that are located along a rear or side facade or within the parking and service zone. Utility meters are prohibited along facades that front a street.

Site Design



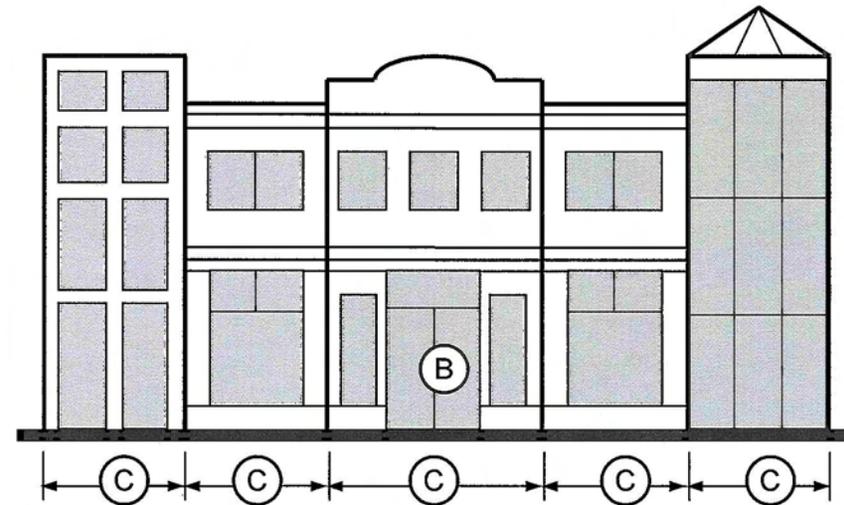
Mass and Height

- A. Buildings shall have a maximum height of two floors.
- B. The elevation of the ground level floor plate shall be built at or near the elevation of the adjacent sidewalk or plaza so that steps or ramps are not required to enter ground floor
- C. Building heights shall not exceed 40 feet, tower elements may project up to 50 feet.
- D. Tower elements may project above roofline of the top floor to provide height variety along the street. Tower elements are required on all corners of buildings that are along the H Street, the Rail Trail, and the West Entry. At least one tower element is also required along H Street near a building entrance (which may be a primary entrance for customers or a service/delivery entrance that is designed to look like a primary entrance).
- E. Upper floor step-backs are allowed.
- F. The following roof forms are allowed
 - Flat Roof with Parapet or Cornice
 - Full Mansard Roof
 - Pyramid Roof (only allowed on tower elements)
 - Shed Roof (only allowed where step-backs occur)
 - Special Roof as approved by the Planning Commission

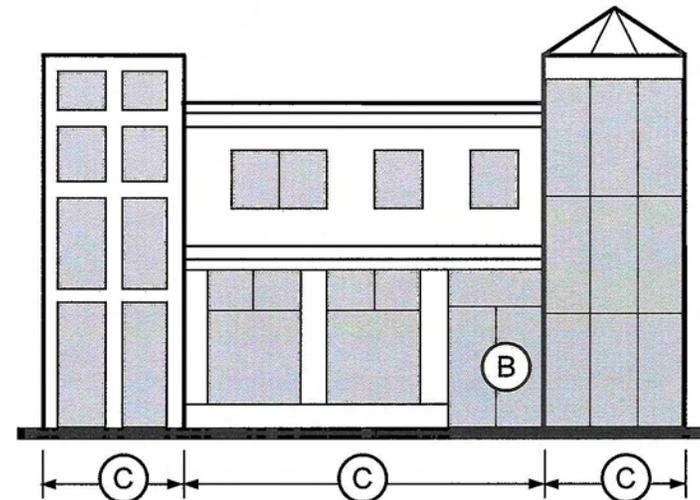


Window Fenestration and Frontages

- A. There is no specific minimum window fenestration required for Family Entertainment Center Buildings. However, windows are encouraged on all facades to the maximum extent feasible
- B. At least one operable entrance is required on building facades along H Street, G Street, the Rail Trail, and parking areas. Entrances may be located at an angle on the corner of two of these areas and count toward the requirement of both.
- C. To articulate the building, the building facade along H Street shall be divided into at least 5 segments. Segments are divisions of building facades that are defined by vertical change in height (such as a tower element) or a projection or recession within the facade.
- D. All other building facades shall be divided into at least 3 segments. Segments are divisions of building facades that are defined by vertical change in height (such as a tower element) or a projection or recession within the facade.
- E. The design of the building facades shall comply with the standards in Section 6 4 (Family Entertainment Facades).
- F. Fences and walls are prohibited between the building and any street. All allowed fences and walls adjacent to the Rail Trail shall be limited to a height of 4 feet. Fences and walls in other locations shall be limited to a height of 6 feet.



Building Facade along H Street



Other Building Facades

Projections

A. The following building elements may extend from ground floor facades:

- Shade Structures
- Projecting Signs
- Covered Walkways

B. The following building elements may extend from upper floor facades:

- Balconies
- Bay Windows
- Shade Structures: Window Shades and Trellises
- Covered Walkways

C. Roof projections and overhangs (excluding covered walkways) may project up to 3 feet from facades

D. A maximum of two covered walkways may be provided over the Rail Trail and between Family Entertainment Center Buildings.

E. All of the above projections may encroach over an adjacent sidewalk, plaza, walkway, or landscaped surface. The above projections shall not encroach over an adjacent private property (except for covered walkways between two Family Entertainment Center Buildings)

F. Standards for the above projections are provided in Section 6 0 (Architectural Design Standards).

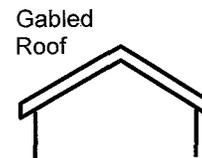
G. If provided, covered walkway connecting between the Family Entertainment buildings shall maintain a minimum vertical clearance of 14 feet between the ceiling of the covered walkway and the ground surface of the walkway and Rail Trail

H. Support posts, beams, and other vertical elements of the covered walkways shall not be located within 2 feet of the pavement along the Rail Trail.

I. Landscaping shall be provided along the edges of the covered walkways. Where covering a plaza space, the landscaping may be included in planters

J. The following roof forms are allowed on covered walkways:

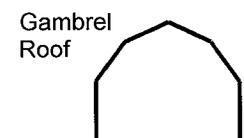
- Gabled or Hip Roof
- Gambrel or Dome Roof
- Pyramid Roof (only allowed on tower elements)
- Shed Roof (only allowed in combination with another)



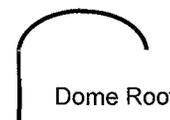
Gabled Roof



Hip Roof



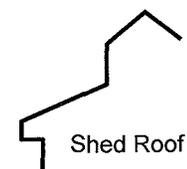
Gambrel Roof



Dome Roof



Pyramid Roof



Shed Roof

Building Uses

A. The following uses are allowed on all floors of the building:

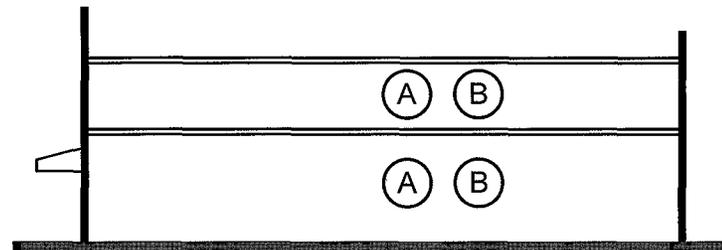
- Family Entertainment Uses

B. The following uses are conditionally permitted regardless of their location:

- Any use that serves or sells alcohol, and is open for business between the hours of 9:00 pm and 7:00 am

C. The following uses are specifically prohibited

- Adult-oriented or adult-only businesses
- Drive-thrus
- Outdoor storage, manufacturing and light/heavy industrial uses



Parking Requirements

- A. Four parking spaces are required for every 1,000 square feet of leasable floor space. Fractional space requirements shall be rounded up to the next whole space.
- B. All of the required parking spaces shall be located within the development site or within a shared parking facility located on the same or adjacent block as the development site
- C. Secured bike parking, provided in the form of bike racks or bike lockers, is required. Bike parking shall be required at a rate of one space per 4,000 square feet of leasable floor space. Bike parking shall be located within parking areas or along pedestrian connections to the Rail Trail

5.11 Public/Civic Buildings

This Regulating Code does not define building standards for public/civic buildings, as these buildings are unique and special and their design will greatly vary based on the intended use of the building. All public buildings within the Rail Trail shall be designed in compliance with the Public Facilities (PF) Zoning District of the Zoning Ordinance. Sections 8.5 and 8.6 provide standards for the design of public spaces that would surround public/civic buildings within the corridor.

5.12 General Standards for Building Types

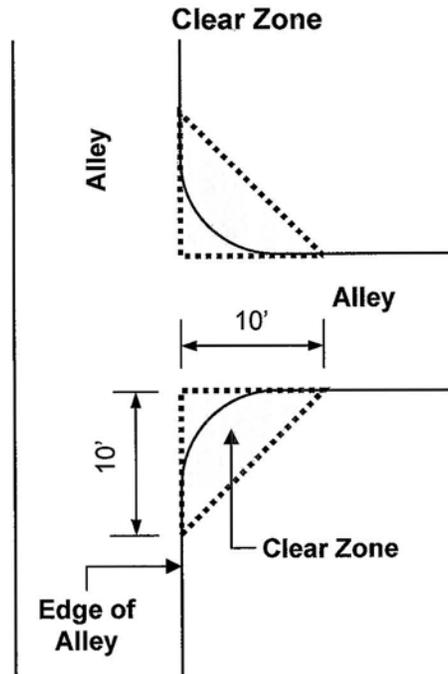
The standards in this section apply to all building types.

Landscaping

- A. All areas of the development site that are not occupied by a building, walkway, or parking area shall be landscaped with drought tolerant plants.
- B. The use of irrigated grass turf is discouraged and should be minimized.
- C. If provided, paseos and courtyards shall be enhanced with decorative pavement and amenities such as pedestrian-scaled lighting, planters, benches, trash/recycling receptacles, and outdoor seating and dining areas.

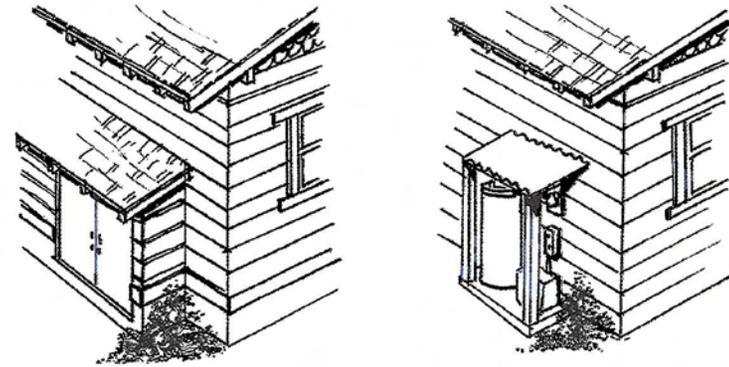
Clear Zones

- A. A clear zone shall be maintained at all alley intersections as illustrated in the diagram to the right. This clear zone may be occupied with a walkway or landscaping. If planted, landscaping in the clear zone shall not exceed 2 feet in height.



Mechanical Equipment

- A. The following items shall be screened from public views from streets, pedestrian walkways, the Rail Trail, sidewalks, plazas, courtyards and public spaces:
- Electric and water utility meters
 - Power transformers and sectors
 - Heating/ventilation/cooling equipment
 - Irrigation pumps
 - Satellite dishes (wider than 18 inches)
 - Antennas
 - Rooftop mechanical equipment
 - Other mechanical equipment
- B Appropriate methods of screening include fencing, building walls, landscaping, roof parapets, and equipment enclosures. The design of screening devices shall be compatible with the main structure and conform to other sections of this Code.



Examples of appropriate (left) and inappropriate (right) methods of screening.

Stormwater Management

- A. Innovative and urban stormwater management designs and techniques shall be used to address stormwater treatment requirements. These designs and techniques include, but are not limited to:
- Porous pavement and permeable surfaces;
 - Treatment inlet boxes with skimmers or traps,
 - Sub-surface basins for infiltration or detention;
 - Infiltration trenches,
 - Catch basin inserts,
 - Prefabricated multi-chamber water quality devices;
 - Bio-retention swales or rain gardens;
 - Underground cisterns and rain barrels, and
 - Green roofs.
- B. All stormwater management designs and techniques shall be certified by a professional engineer that is competent in hydrology, drainage, flood control, and stormwater management.
- C. A maintenance schedule shall be prepared for all stormwater management techniques used on the development site. The maintenance schedule will identify the timing of inspections and maintenance activities that are required, including removing debris from inlet boxes, replacing filters, pumping out accumulated sediment, and mechanical sweeping.
- D. Property owners and/or homeowner's associations shall be responsible for all maintenance activities. Homeowner's Association fees shall include the associated costs to implement the maintenance schedule for stormwater facilities.
- E. Public spaces within the Rail Trail Corridor may be designed as stormwater facilities for adjacent development sites. However, the use of these public spaces as stormwater facilities shall not reduce their functionality as public spaces. All public spaces that function as stormwater facilities shall be designed to meet the applicable standards in Section 8.0 (Public Spaces)

Lighting

A. The following areas shall be illuminated at night to ensure the safety of users and to minimize opportunities for crime

- Walkways, pedestrian cut-throughs, and paseos,
- Courtyards, plazas, and outdoor seating areas,
- Sidewalks;
- Surface parking lots and tuck-under parking spaces;
- Parking structures and podium parking areas (including access points, elevators, and stairwells),
- Automated teller machines (ATMs);
- All entrances to buildings and units (including rear, side, and service entrances);
- Garbage disposal and service areas;
- Alleys, and
- Other areas that are routinely used by pedestrians,

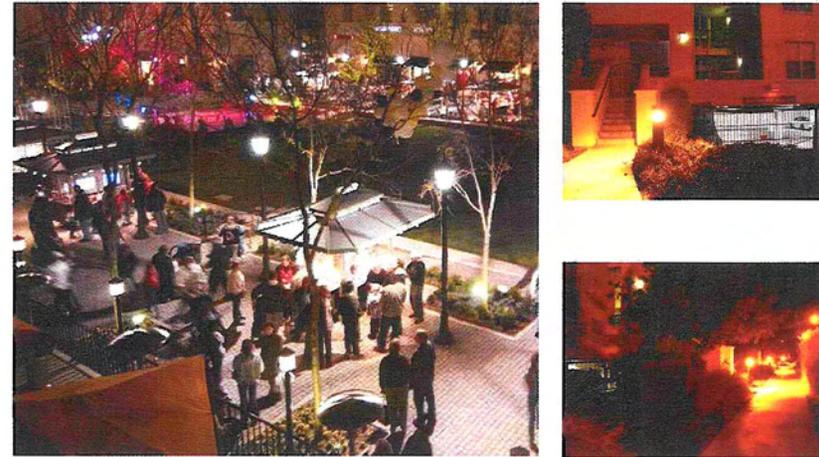
B Site, building, and sign lighting shall be located and directed to light the intended area of illumination and to prevent off-site glare impacts on adjacent buildings and properties.

C. Over-lighting of buildings and sites shall be avoided. Over-lighting can create an environment that feels like a prison-yard and can ruin desired nighttime ambience.

D. Lighting poles and fixtures may have a decorative and/or ornamental design that complements the structures on site.

E. Lighting within storefront windows shall be provided to showcase merchandise in the storefront, illuminate the sidewalk, and create a desirable nighttime ambience.

F Lighting shall be provided at regular intervals to prevent the creation of light and dark pockets. Dark pockets can create uncomfortable areas for pedestrians and provide opportunities for criminals to hide in dark shadows. Light pockets can create a “fish bowl” affect. Within the light pocket (or the “fish bowl”), pedestrians may be observed, but their ability to see outside of the light pocket is limited, which creates discomfort and insecurity.



Examples of appropriate nighttime lighting



Examples of inappropriate nighttime lighting

5.13 In-Lieu Parking Program

All mixed-use, commercial, and live-work development sites within the Rail Trail Corridor shall be eligible to participate in the In-Lieu Fee Parking Program per the requirements provided below:

Fee Collection Process

Fees for all development projects participating in the In-Lieu Fee Parking Program shall be paid to the City prior to issuance of building permits. Upon receipt of the fee, the City shall place the fee in a parking fund reserved for public parking improvements within the Rail Trail Corridor. The City will maintain a record of all properties that have met their required parking space obligation by paying the appropriate fee for the spaces. Payment of this fee does not guarantee that parking spaces will be constructed for the sole use of or in the immediate proximity of that development.

Amount of Fee

The fee for each required parking space shall be fixed by resolution adopted from time to time by the City Council, but in no case shall the fee exceed the estimated current cost to the City of providing required parking spaces to serve the contemplated use. The resolution will include allowances to increase the fee based on an established cost index.

Fees paid in accordance with this Code are collected to fund a general public parking program serving the Rail Trail Corridor and are not intended for any specific improvement project. The fees paid shall be the most current fees as established by the City Council.

Change or Cessation of Ownership and/or Use

The following apply to changes in use, ownership and development of properties for which in-lieu parking fees have been used to satisfy parking requirements pursuant to this Ordinance:

- Any satisfaction of parking requirements through the payment of fees under this Program shall continue to apply to the subject property in the event of a change in ownership.
- Any change of use of a property, or building addition or redevelopment of a site, that requires more parking may trigger additional parking requirements. The number of spaces required by the change in use shall be the difference between the number required by the new use or structure and the number required by the previous use or structure.
- If a structure is reduced in area, or wholly or partially becomes vacant, or a use is replaced with a use for which less parking is required according to this Regulating Code, there shall be no change in the obligation for parking in-lieu fees. There shall be no refund of in-lieu parking fees. Once satisfied, the obligation for parking in-lieu fees shall be deemed to meet the parking requirement equal to or less than that for which the in-lieu fee amount was previously determined.

Use of Fee

The fees collected shall be used for constructing, maintaining, or providing new public parking spaces within, or for the direct benefit of the Rail Trail Corridor. Eligible projects include:

- Purchase of land for parking facilities,
- Planning and design of parking facilities;
- Construction of a parking structure;
- Construction of temporary public parking lots. Temporary public parking lots are allowed on all blocks within the Rail Trail Corridor;
- Construction of permanent public parking lots. Permanent public parking lots are allowed on all blocks that are identified as Block Type G on Figure 4-1 (Block Types). They are also allowed on Block Types C and D but only if buildings are located between the parking lot and all streets and plazas within and adjacent to the block; and
- Maintenance of public parking facilities created through this program.

Trust Fund

An interest-bearing trust fund shall be maintained exclusively for the Parking Fund. These trust funds and interest earned by these trust funds shall be used solely for improvements identified in "Uses of Fees" above.

Termination

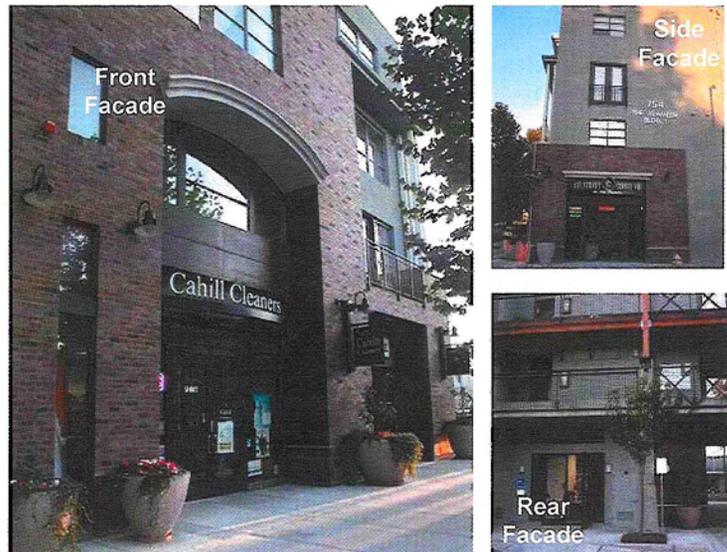
If this fee program is terminated, any excess funds collected prior to dissolution of this fee program shall be used for those purposes identified in "Uses of Fees" above.

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6.0 Architectural Design Standards

6.1 Facades (General)

- A. External stairways going to the upper floors of a building (which does not include stoops) and external hallways are prohibited along facades that face streets and public spaces. This standard is required to prevent building facades that look like motels.
- B. All building facades shall be designed with similar architectural elements, materials, and colors as the front facade. However, the design of side and rear facades may be simpler, more casual, and more utilitarian in nature.



Appropriate relationship between front, side, and rear facades on a mixed-use building



Appropriate relationship between front, side, and rear facades on a residential building

6.2 Facade Articulation

A. Facades shall be articulated to improve the quality of the design. Appropriate methods of articulation include, but are not limited to

- Changing the direction of the wall or facade;
- Stepping back an upper floor,
- Increasing the number and/or size of window openings;
- Altering the height of the building or roofline;
- Breaking up large smooth surfaces with expansion joints, expression lines, reveals, or changes in texture and color;
- Dividing large window openings by using smaller window panes;
- Providing projecting awnings or shade structures over window openings;
- Adding depth and detail to the cornice or roof parapet,
- Recessing storefronts, building entrances, and windows into the facade to create depth and cast shadow patterns;
- Creating a defined building cap or roofline,
- Providing stylized windows and doors;
- Creating a defined base for the building;
- Providing expression lines between the floors of the structure.



6.3 Parking Structure Facades

- A. Parking structures are allowed only on Mixed-Use development sites (see Figure 3-1: Regulating Plan).
- B. If a facade of a parking structure is visible from the Rail Trail or the Fifth Street Public Parking Lot, it shall be designed to be visually compatible with other building facades on the development site, and not clearly look like a parking structure. Parking structure facades shall be designed with window patterns, materials, details, and colors that are similar to adjacent facades.
- C. If feasible, sloped ramps within a parking structure should not be visible along any facade.



Examples of inappropriate parking structure facades



Examples of appropriately designed parking structure facades

6.4 Family Entertainment Facades

A. These standards only apply to all facades on Family Entertainment Center Buildings

B. Facades shall be designed with traditional elements that are typically found on "Main Street" buildings, such as

- Ground floor storefronts (see Section 6.10);
- Recessed entrances;
- Building piers, columns, or posts;
- Cornices or horizontal expression lines defining the top of the storefront;
- Upper floor windows, and
- Parapet roofs.

These elements may be reinterpreted to create a more contemporary building front



Examples of family entertainment facades

C. Windows are strongly encouraged along all facades. In areas where windows are not feasible because of the interior building use, the facade shall be articulated with details and facade elements to prevent blank walls. Detailed facade elements may include:

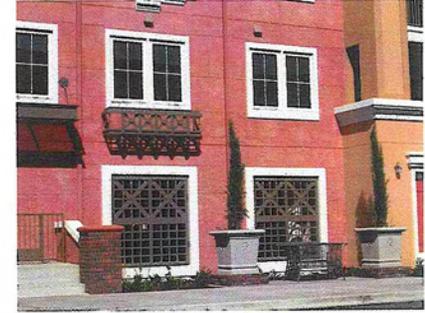
- Recessed building bays divided by columns,
- Vertical expansion joints that transition between building colors and/or materials;
- Grates, trellises, and similar features that project from the wall and that may also include landscaping; and
- Varied rooflines and parapet patterns between different building segments



Examples of articulated facades with portions that do not have windows

6.5 Podium Parking

- A. If used, podium parking shall be screened from sidewalks, walkways, courtyards, and public spaces by using terraced planters with landscaping, decorative trellis screens over window openings, and/or stairs, front porches, or stoops.
- B. The facade of podium parking shall consist of a solid wall surface that is compatible with the rest of the building facade. Window openings are allowed to provide ventilation and light to the podium parking level. Window openings shall not exceed 5 feet in height or 4 feet in width. Window openings shall contain metal screens (excluding chain link fencing). Metal screens shall be designed with vertical orientation. Podium parking facades that do not contain a solid surface are prohibited.



Examples of podium parking facades with solid surfaces that are compatible with the rest of the building facade and metal screens for partial underground podium parking (left) and at-grade podium parking (right)

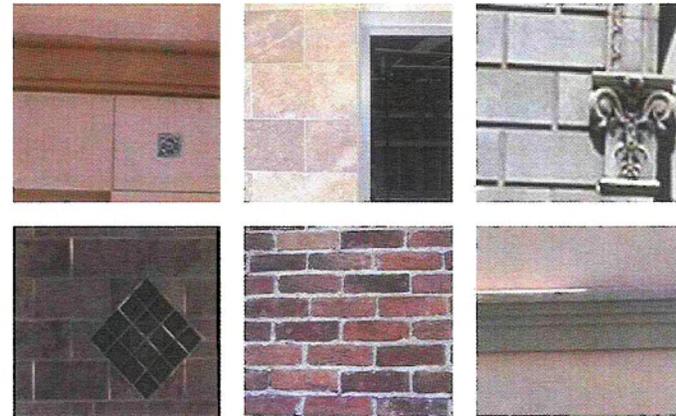


Example of podium parking facade without a solid surface (prohibited)

6.6 Building Materials

A. Finish materials shall be complimentary to one another and appropriate for the architectural style and character of the building. Appropriate finish materials include:

- Plaster and stucco;
- Poured in place concrete,
- Fiber cement;
- Glass;
- Brick (unglazed),
- Stone and marble block;
- Ceramic tiles (as an accent material);
- Wrought iron (in storefronts);
- Wood siding;
- Hardie board;
- Finished and painted wood trim,
- Metal posts, frames, trellises, and canopies; and
- Wood, aluminum, and steel framed windows and doors



Examples of appropriate building materials

B. Inappropriate finish materials that are prohibited include

- Concrete block;
- Plywood;
- Hardboard,
- Unfinished lumber;
- Corrugated fiberglass siding;
- Vinyl or aluminum siding;
- Corrugated sheet metal or tin siding;
- Textured T-11 siding; and
- Highly reflective materials.

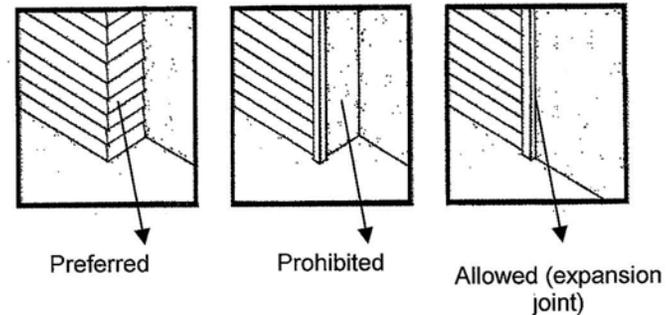
C. Building materials shall be durable and able to withstand long-term exposure to the hot sun and other environmental conditions of the City

D. If used, bricks shall be unglazed and in stacked with a horizontal orientation

E. If used, brick and stone veneer shall be mortared to give the appearance that they have a structural function.

F. If used, brick and stone materials (including veneer) shall wrap around corners to give an appearance of structural function and minimize a veneer appearance.

G. Changes in material shall occur when there is a change in the plane of the facade. The change in material and/or color shall occur on inside corners of the building. If a change is proposed along the line of a single plane, a pronounced expansion joint shall be used to define a clear separation. Changes in material and color shall never occur on an outside corner.



6.7 Building Colors

- A. Buildings shall be painted with muted and soft colors that are complementary to one another and appropriate for the architectural style and character of the building. Bright and bold colors may be used as accents, to highlight key building features, and to add diversity to the building. Extensively bold, bright, fluorescent, and neon colors are prohibited.
- B. Building facades may have one or two main colors and up to three accent colors that compliment the main color(s) of the building.
- C. Painted building surfaces shall have a matte finish. Trim work may have a glossy finish.
- D. The natural colors of brick and stone material shall be maintained. These materials shall not be painted or glazed.



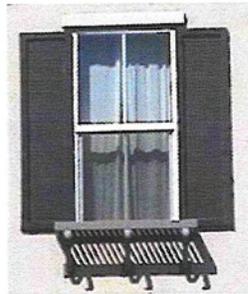
Examples of appropriate colors



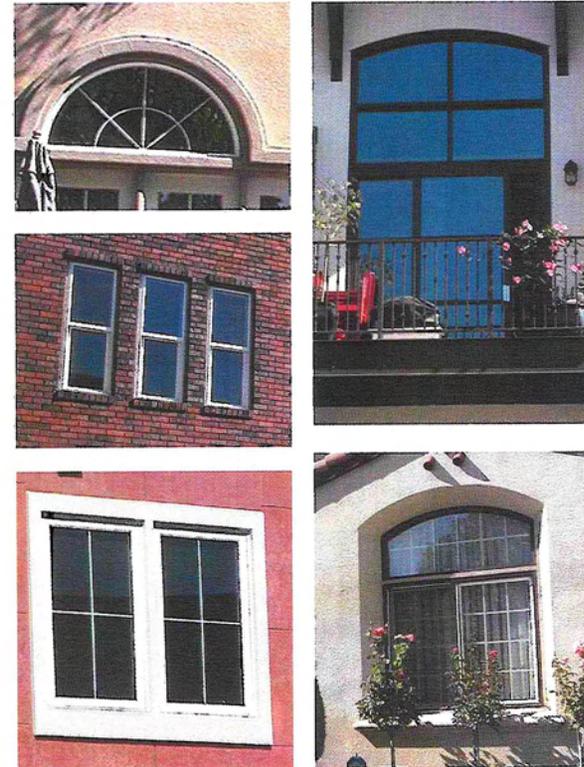
Examples of inappropriate colors

6.8 Windows

- A. Windows shall have basic and simple shapes (such as rectangular, square, and semi-circular shape)
- B. Window openings shall have a vertical orientation and proportion. Window openings with square or horizontal orientation are allowed only if the windows and windowpanes have a vertical orientation.
- C. If used, window and door shutters shall have a width that would enclose the entire window or door opening when the shutters are closed.
- D. Windows may be recessed into the wall surface to help articulate the facade and to create interesting shadow patterns.
- E. Windows that open and close shall be used to encourage natural ventilation of the building and less dependence on energy to ventilate, heat, and cool the structure.



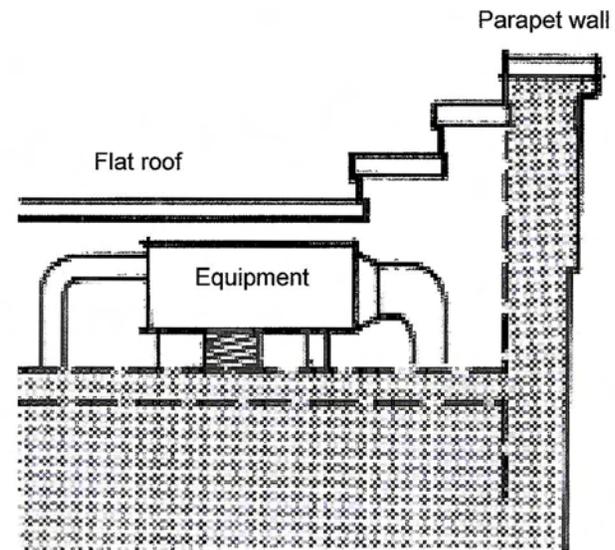
Examples of shutters that would cover the window if shut



Examples of basic and simple window shapes

6.9 Roofs

- A. Roofing forms, slopes, details, and materials shall be compatible with the overall style and character of the building. See Section 5.0 (Site and Building Standards) for appropriate roofing forms by building type.
- B. A compatible and relatively consistent roof design (including overhangs, pitch, materials, fascia, and eaves) shall be provided on all sides of the building.
- C. All roofs shall be designed to prevent water damage and stains on building facades and to protect pedestrians from dripping water. If provided, gutters and downspouts shall drain directly into a cistern, landscaped area, retention or detention basin, bioswale, or storm drain system.
- F. Appropriate types of roof materials that are required on buildings include:
- Slate or fiber cement shingles;
 - Clay or concrete tile;
 - Composite roofing materials made of recycled natural fiber and/or recycled plastic, and
 - Tar, gravel, composition, or elastomeric roof materials should only be allowed on flat roofs that are concealed by a parapet or cornice feature.
- G. Roof overhangs may project up to 2 feet from facades and may encroach into the setback and over an adjacent sidewalk or walkway if a minimum of 8 feet of vertical clearance is provided.
- H. Green roofs and rooftop gardens are permitted to add landscaping, decrease the heat island effect of large expanses of flat roofs, and to reduce energy demand for heating and cooling buildings.
- I. Solar panels are allowed and encouraged on roofs.
- J. Mechanical equipment on roofs (excluding solar panels) shall be screened from public views from all sidewalks, walkways, public spaces, and the Rail Trail.



Example of mechanical equipment on flat roof screened from public view

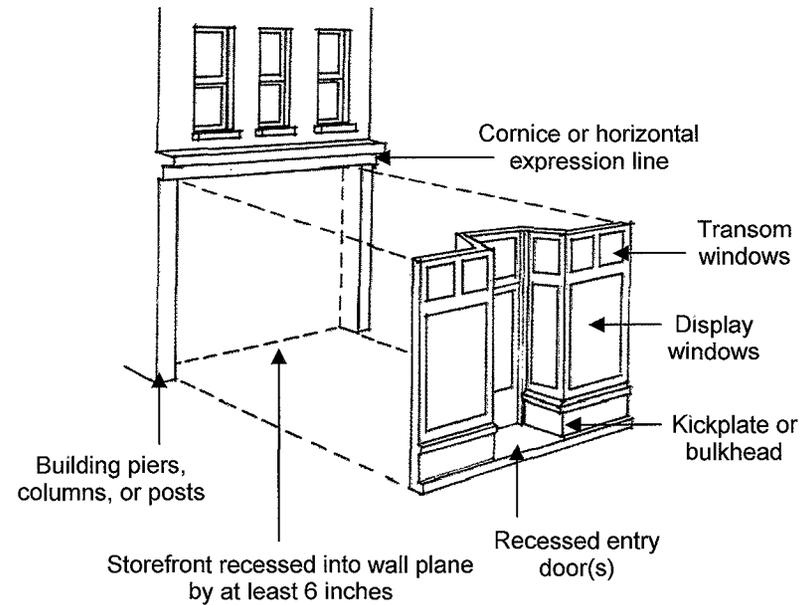
6.10 Storefronts

A. Storefronts shall be designed with traditional elements that are found on typical "Main Street" buildings, such as:

- Recessed entry doors;
- Large display windows,
- Transom windows,
- Kickplates or bulkheads;
- Building piers, columns, or posts, and
- Cornices or horizontal expression lines defining the top of the storefront.

These elements may be reinterpreted to create a more modern storefront.

B. Storefronts shall be recessed into the wall plane by at least 6 inches and shall be framed by building piers, columns, or posts, and a cornice or horizontal line.



6.11 Balconies

- A. Balconies are permitted to provide outdoor spaces for residents and office employees
- B. Balconies may project from the facade or be recessed into the facade. Balconies may project up to 4 feet from the facades and may encroach into the setback and over the adjacent sidewalk or walkway if a minimum of 8 feet of vertical clearance is provided.
- C. Balconies may be open or covered with a roof or shade structure.
- D. The design of balconies (including railings, balustrades, posts, columns, details, and roof covering) shall be constructed of materials, colors, and details that are compatible with the style of the building and the materials used on the facade. The balcony shall not have a tacked-on appearance or look like it was an addition or afterthought.
- E. The sides of balconies that project from a facade shall be designed with posts, railings, and balustrades rather than a solid wall plane.



Example of a balcony covered by a shade structure



Example of an open projecting balcony



Examples of balconies recessed into the facade

6.12 Shade Structures

- A. Shade structures include awnings, marquees, window shades, and trellises.
- B. Awnings and marquees may project up to 5 feet from facades and may encroach into the setback and over the adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet).
- C. Window shades and trellises may project up to 4 feet from facades and may encroach into the setback and over the adjacent sidewalk or walkway (with a minimum vertical clearance of 8 feet)
- D. Valances (or front face of an awning) shall not exceed 18 inches in height.
- E. Shade structures shall not cover storefront display windows, piers, columns, pilasters, architectural expression lines, or other details.
- F. Awnings and marquees may have signs (see Section 6 15: Commercial Business Signs, below)
- G. Shade structures shall be designed with materials, shapes, and colors that are compatible with the style, character, materials, and colors of the facade.

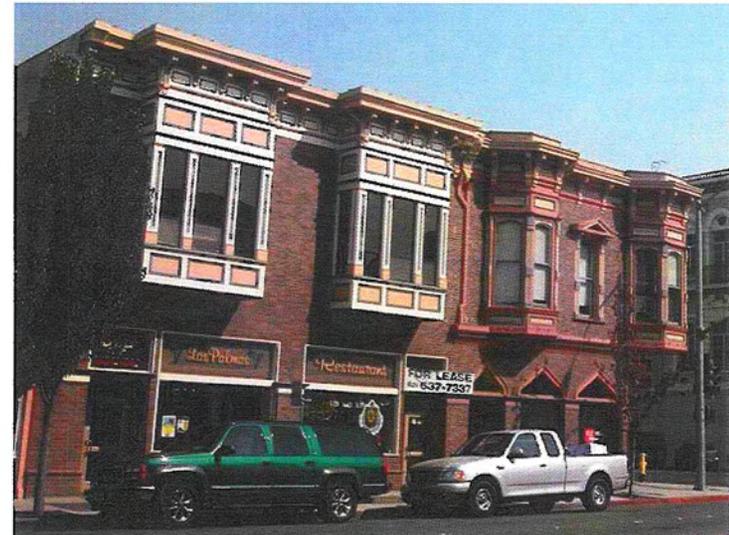
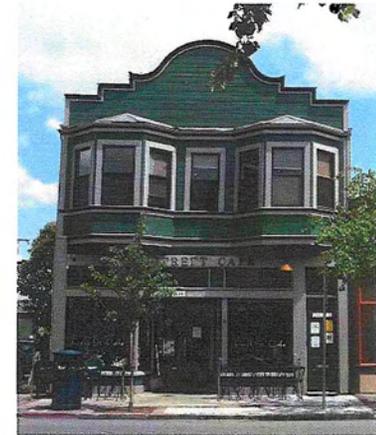
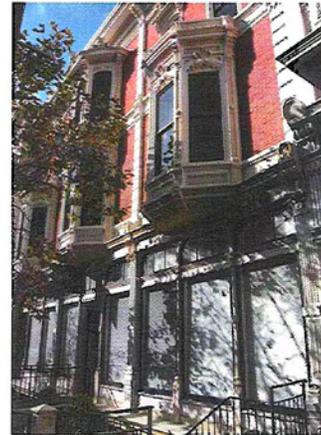
- H. All awnings on a building shall have a consistent size, shape, and placement pattern. However, awnings over primary building entrances may be larger and more pronounced than other awnings on the facade. Awnings on a building may have different colors and patterns to represent the different businesses in the building.
- I. Awnings shall have basic and simple color patterns, such as one solid color or a vertical striping pattern with two colors
- J. Awnings shall be made of colorful canvas and fabric or other materials that are complimentary to the facade and roofing materials. Backlit awnings are prohibited



Examples of appropriate awnings, marquees, and window shades

6.13 Bay Windows

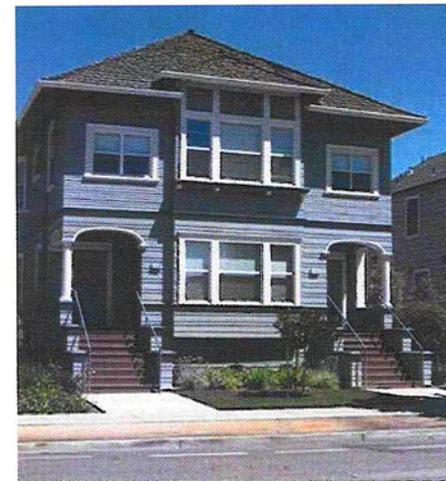
- A. Bay windows shall not exceed a width of 10 feet.
- B. Bay windows shall be designed with materials, colors, and details that are compatible with the style and character of the building and the materials and colors used on the facade
- C. If more than one bay window is provided on a facade, at least 4 feet of horizontal separation shall occur between the bay windows.
- D. Windows shall be provided on all sides of the bay window and shall have a vertical orientation and proportion



Examples of appropriate bay windows

6.14 Front Porches and Stoops

- A. Front porches shall be covered with a roof, balcony, or enclosed habitable space. An enclosed habitable space is only allowed above a porch if the porch does not encroach into the front setback
- B. Front porches shall not be enclosed by permanent or temporary walls, windows, window screens, or plastic or fabric materials.
- C. Front porches and stoops may project up to 6 feet from facades and may encroach into the setback.
- D. The raised platform of a front porch (not including stairways) shall be at least 48 square feet in size with no dimension less than 6 feet in length.
- E. The raised platform of a stoop (not including stairways) shall be at least 20 square feet in size with no dimension less than 4 feet in length.
- F. The design of a front porch or stoop (including materials, colors, and details) shall be compatible with the overall style, character, and form of the building. The front porch or stoop shall not have a tacked-on appearance or look like it was an addition or afterthought
- G. The stairs of a porch or stoop shall be boxed or framed. Floating stairways are prohibited.

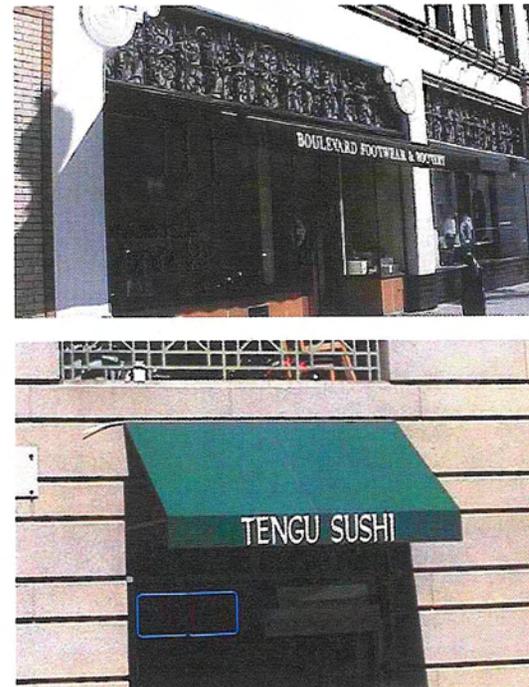


Examples of an appropriate front porch (top) and stoop (bottom).

6.15 Commercial Business Signs

- A. Commercial business signs are only allowed on mixed-use buildings, commercial buildings, and live-work buildings
- B. In order to avoid sign clutter, signage shall only be allowed on facades that have building entrances.
- C. Buildings shall be designed with appropriate locations for signs. Signs shall not cover or obscure windows, doors, storefronts, building entrances, cornices, columns, or other architectural elements or details
- D. The gross area of all signs that are mounted parallel to a facade shall not exceed 10% of the total area of the facade.
- E. The following types of signs are permitted:
- Awning Signs
 - Storefront Signs
 - Building/Primary Tenant Signs
 - Window Signs
 - Projecting Signs
 - Directory Signs
- Standards for these signs are provided below.

- F. Awning Signs: A sign that is printed or mounted on an awning. The following standards apply to these signs
- Signs shall only be allowed on the front face or valance of the awning.
 - Lettering shall not exceed a height of 8 inches
 - The width of the sign shall not exceed 80% of the width of the awning.
 - Awnings (and associated signs) shall provide a minimum vertical clearance of 8 feet between the bottom of the awning and the top of the sidewalk or walkway.



Examples of awning signs

G. Storefront Signs: A horizontally oriented sign that is mounted on the facade above the entrance to ground floor stores. The following standards apply these signs

- Storefront signs shall not project out from the facade more than 12 inches.
- Storefront signs shall not exceed a height of 24 inches.
- Ground floor businesses within a building may have one or multiple storefront signs. However, the cumulative width of all storefront signs on a facade shall not exceed 50% of the facade's width
- Storefront signs shall be placed in an area that is above the ground floor storefront windows and below the windows on the second floor
- Storefront signs may be illuminated externally by lights mounted on the facade, by backlighting behind individually mounted letters or symbols ("halo" lighting), by neon tubes, or by the internal illumination of sign symbols and letters. Internally illuminated box signs that light the entire sign (letters, symbols, logos, and background) are prohibited.



Backlighting behind individually mounted letters (permitted)



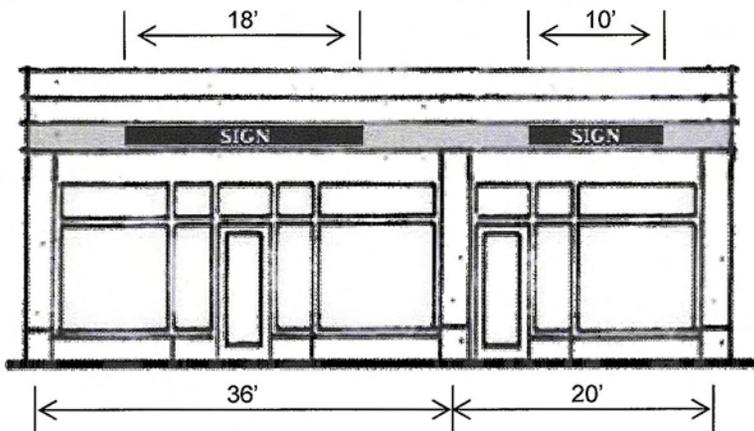
Externally illuminated sign (permitted)



Internal illumination of symbols and letters but not the sign background (permitted)



Internally illuminated box sign (prohibited)



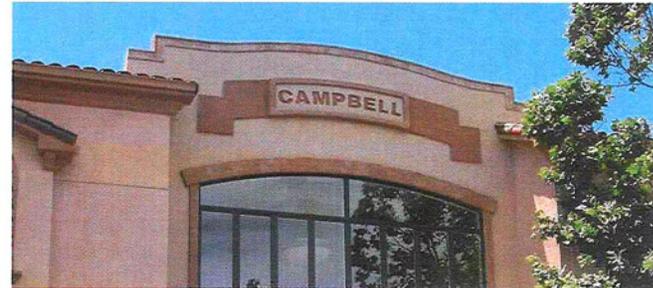
Example of appropriate measurements for storefront signs



Example of a storefront sign

H. Building/Primary Tenant Signs. A horizontally oriented sign that is mounted above the windows of the top floor of the building. These signs are used to identify the name of the building or the primary tenant of the building. The following standards apply to these signs.

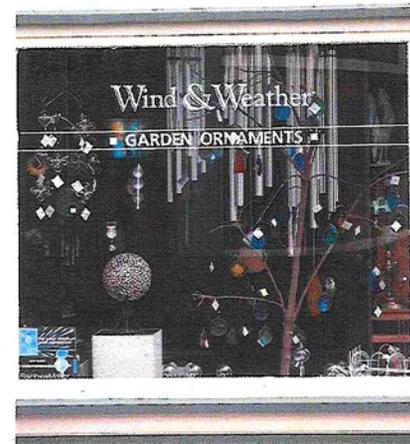
- Only one of these signs shall be permitted per building
- The distance between the lowest point and highest point of a building/primary tenant sign shall not exceed 36 inches.
- Building/primary tenant signs shall not project out from the facade more than 12 inches
- Building/primary tenant signs may be illuminated externally by lights mounted on the facade, by backlighting behind individually mounted letters or symbols ("halo" lighting), or by the internal illumination of sign symbols and letters. Internally illuminated box signs that light the entire sign (letters, symbols, logos, and background) are prohibited.



Examples of building/primary tenant signs

I. Window Signs: A temporary or permanent sign that is placed on or within 18 inches of a window (including windows on doors) Window signs include posters for advertisements and sales, product merchandise posters, open and closed signs, and painted or etched business names and logos The following standards apply to these signs:

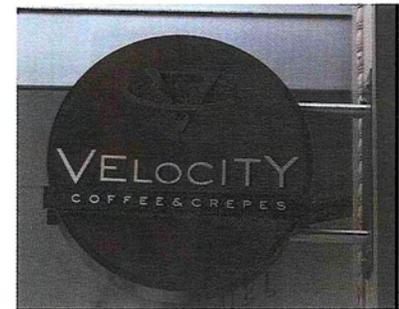
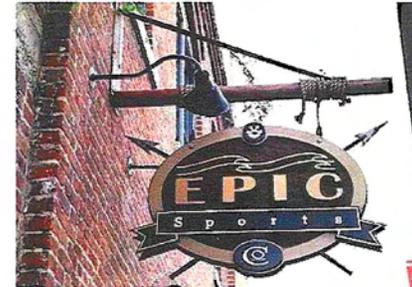
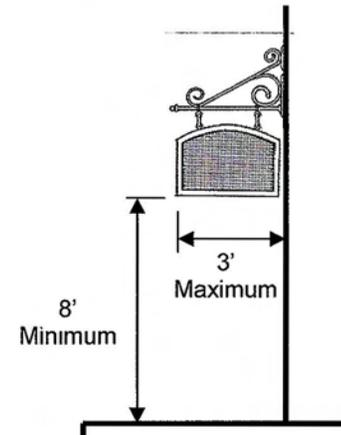
- Window signs shall only be used on windows for non-residential uses. Window signs may be used on ground floors of live-work units and in residential sales/leasing offices.
- Window signs shall not occupy more than 25% of a window.
- Permanent window signs shall be created with permanent or fade resistant paint, gold-leaf lettering, or glass etching
- In-store lighting and/or an externally mounted light shall be used to illuminate window signs. The use of neon tubing should be limited to “open” signs.



Examples of window signs that are no more than 25% of the window space

J Projecting Signs A double-sided sign that projects perpendicular to the building facade and hangs from a mounted wall brace or from the ceiling of a balcony or arcade. The following standards apply to these signs.

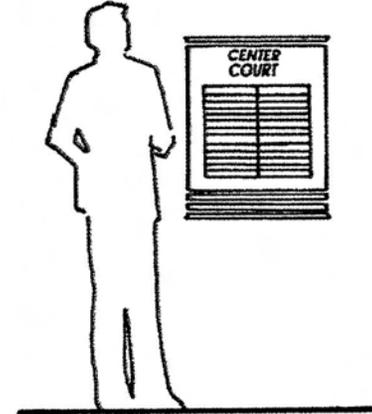
- Projecting signs may project up to 3 feet from a facade.
- At least 8 feet of vertical clearance shall be provided from the lowest point of the sign to the sidewalk or walkway
- The maximum area of a projecting sign shall not exceed 10 square feet.
- Projecting signs shall be mounted near storefront entrances
- The top of projecting signs shall be located below the windows on the second floor of the building.
- A maximum of one projecting sign shall be permitted for every storefront entrance on the facade
- Projecting signs shall be externally illuminated by a light mounted on the facade or support base.



Examples of projecting signs

K. Directory Signs: A small sign that is attached flat against the facade at the eye level of pedestrians. Directory signs are either used to identify an individual business within a storefront or to identify multiple tenants that are accessible by a shared entrance or lobby. The following standards apply to these signs:

- Directory signs shall only be used near building entrances
- Directory signs shall not exceed an area of 6 square feet.
- Only one directory sign shall be permitted for each storefront or lobby entrance.
- Directory signs shall be externally illuminated. Internal illumination and neon lighting is prohibited.



Examples of directory signs

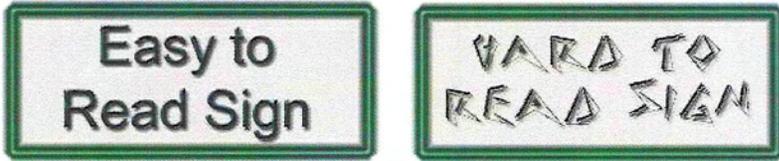
L. The following types of signs are prohibited:

- Pole mounted or “lollipop” signs,
- Billboards,
- Inflatable or windblown signs;
- Signs that produce smoke or sound;
- Signs with animated or moving characters;
- Changeable letter marquee signs (except for theaters, concert venues, and public facilities),
- Window signs that occupy more than 25% of the window’s area;
- Roof mounted signs,
- Permanent sidewalk signs, and
- Monument signs (except for public facilities and gateway entrances).



Examples of inappropriate signs

- M. Signs shall be constructed of durable and weatherproof materials so that they will not discolor, fade, crack, rust, or erode.
- N. Simple and easy-to-read typefaces shall be used on signs. Hard-to-read and intricate typefaces are prohibited.



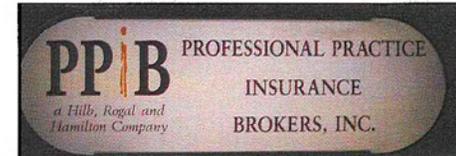
Examples of an easy to read sign (left) and hard to read sign (right)

- O. Signs that have symbols, characters, or graphics may be used. The symbol, character, or graphic shall relate to the products sold in the business or to the name of the business



Examples of signs that symbolize what is sold in the store

- P. Signs that show depth and cast shadows may be used. Depth and shadows can be created by mounting individually cut letters and symbols on the sign base or carving letters and symbols into the base of the sign
- Q. Sign materials and colors shall compliment the building facade. Basic and simple color applications shall be used. Vibrant and fluorescent colors are prohibited.
- R. The color of letters and symbols shall contrast the base or background color of the sign to maximize readability
- S. Sign lighting shall be directed and shielded to illuminate the sign and not to spill over to other parts of the building or site.
- T. Internally illuminated box signs that light the entire sign (letters, symbols, logos, and background) are prohibited.



Internally illuminated signs that illuminate the entire sign are prohibited



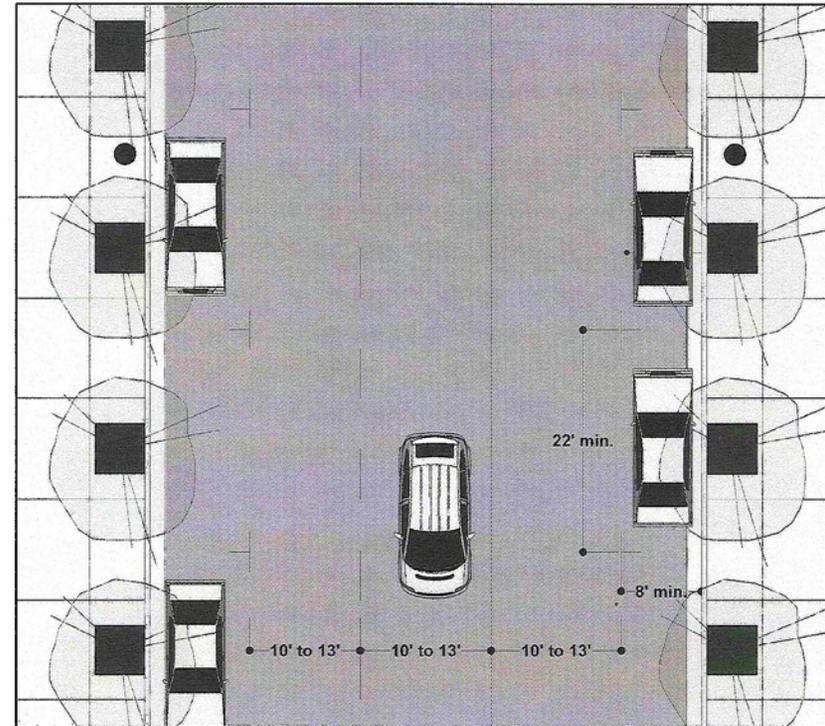
Internally illuminated signs that illuminate letters and symbols but not a background

7.0 Streetscape Standards

To create a more cohesive and attractive street environment that encourages connectivity and pedestrian activity, the existing streets within the Rail Trail Corridor shall be improved consistent with the standards in this section. New streets shall also be constructed in compliance to these standards.

7.1 General Streetscape Standards

- A. All utility lines in the Corridor shall be located underground. Existing utility lines shall be placed underground as properties along the corridor redevelop and adjacent street improvements are constructed.
- B. Public sidewalks are required on both sides of the street to create a pedestrian-friendly environment. Sidewalks constructed or improved shall be consistent with the standards in Section 7.2 (Sidewalk Standards) below.
- C. Streets, sidewalks, crosswalks, sidewalk amenities, outdoor dining areas, and all associated elements shall be designed consistent with the Americans with Disabilities Act (ADA).
- D. On-street parking is required on both sides of the street to provide convenient customer and guest parking, help calm traffic, and to provide a buffer between pedestrians on the sidewalks and moving vehicles in the travel lanes. On-street parking spaces shall be parallel to the sidewalk; angled spaces are prohibited. On-street parallel spaces shall be 8 feet wide (as measured from the curb face) and shall be at least 22 feet long.

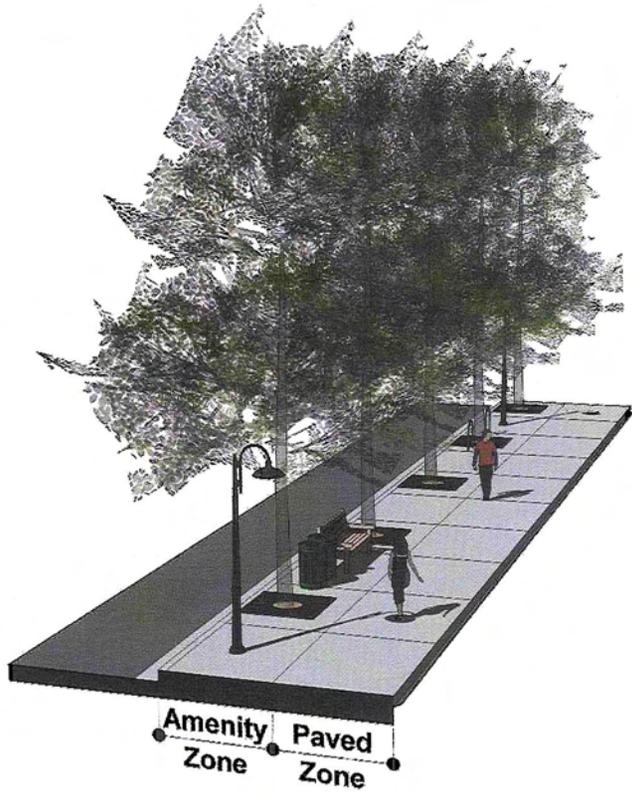


On-street parking configurations

7.2 Sidewalk Standards

Amenity and Paved Zones

- A. Sidewalks shall be divided equally into two zones: an amenity zone and a paved zone. The amenity zone shall be located adjacent to the street and the paved zone shall be located between the amenity zone and private property.
- B. Street trees, streetlights, sidewalk amenities, and public utility equipment shall be located in the amenity zone. If the adjacent building has ground-floor commercial uses, the surface of the amenity zone shall consist of concrete or brick pavers and metal tree grates. If the adjacent building has ground-floor residential uses, the surface of the amenity zone shall consist of decomposed granite or soil with drought tolerant landscaping. If the adjacent building has other uses (live-work, public/civic, or family entertainment), the surface of the amenity zone may consist of concrete or brick pavers and metal tree grates, or decomposed granite or soil with drought tolerant landscaping. See "Sidewalk Amenities" below for additional standards.
- C. The paved zone of the sidewalk shall consist of a sidewalk paved with concrete or brick pavers. Items that may block the path of pedestrians (i.e. sidewalk amenities, public art, outdoor dining, and planters) are prohibited in the paved zone unless they are in compliance with "D" and "E" below.



Commercial uses on ground floor of adjacent building (amenity zone contains brick pavers or concrete and metal tree grates)

Other uses on the ground floor of the adjacent building may include either brick or concrete pavers and tree grates or decomposed granite or soil with drought tolerant landscaping



Residential uses on ground floor of adjacent building (amenity zone contains decomposed granite or soil with drought tolerant landscaping)

D. Sidewalk amenities, public art, outdoor dining facilities (also see "E" below), planters, and other items that may block the path of pedestrians shall only be permitted within the paved zone based on the following standards (as illustrated on the following pages), and the "Sidewalk Amenities" standards below:

- The above items shall only be permitted in the paved zone if an unobstructed flat walking surface (sidewalk and/or tree grate) is maintained along the sidewalk. The unobstructed flat walking surface shall be at least 5 feet wide,
- Items placed within the paved zone shall not block a building entrance or path leading to a building entrance,
- Items placed within the paved zone shall not be permanently attached to the building or the sidewalk, and
- Items placed within the paved zone shall not block views into adjacent storefronts

E Outdoor dining shall require an Administrative Permit to be issued by the Planning Director. Outdoor dining areas shall only be permitted within the paved zone based on the following standards and the graphics on the following pages.

- Outdoor dining areas must comply with the standards in "D" above
- Outdoor dining is only allowed directly in front of the business serving the food.
- If an outdoor dining area occupies an area that extends more than 3 feet from the building facade, a dining area barrier shall be used to define the edges of the dining area. Dining area barriers shall be 36 to 42 inches tall. The dining area barrier may be a sectional freestanding metal or wood fence, freestanding posts connected by a rope or chain with a maximum diameter of 1 inch, or a group of planted pots or planter boxes that surround the dining area. Chain link fences and fences with fabric inserts are prohibited.
- Tables, chairs, umbrellas, trash/recycle receptacles, and outdoor heating devices (if approved by the Fire Department) may be located in outdoor dining areas. Service stations, bars, counters, shelves, racks, and sofas are prohibited in outdoor dining areas
- Umbrellas, outdoor heating devices, and other elevated/raised items shall maintain a vertical clearance of at least 7 feet

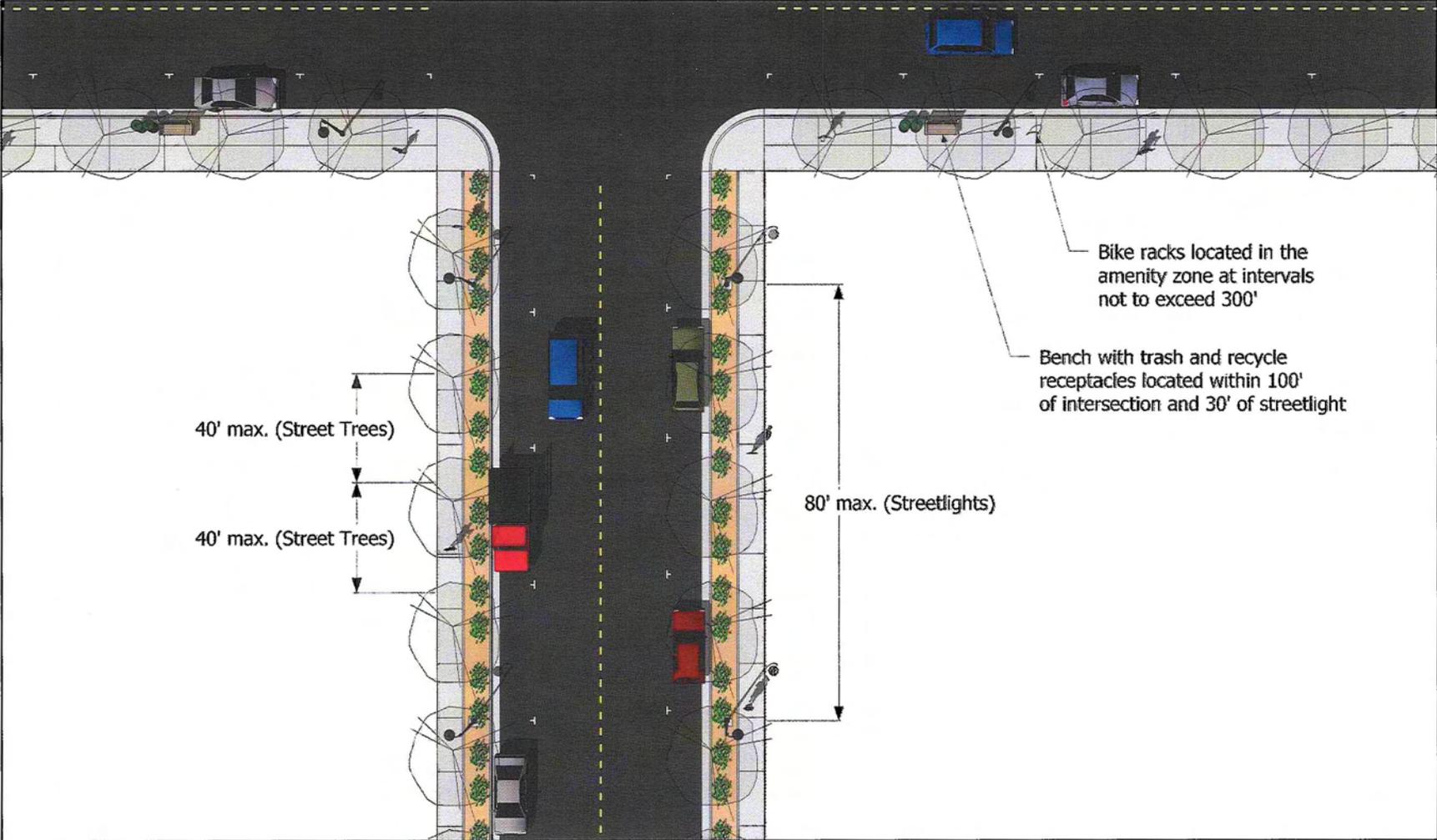


Sidewalk configuration without outdoor dining

Sidewalk Amenities

Sidewalk amenities shall be provided in compliance with the following standards (as illustrated on the graphic on the following page), and the standards in “Amenity and Paved Zones” section above

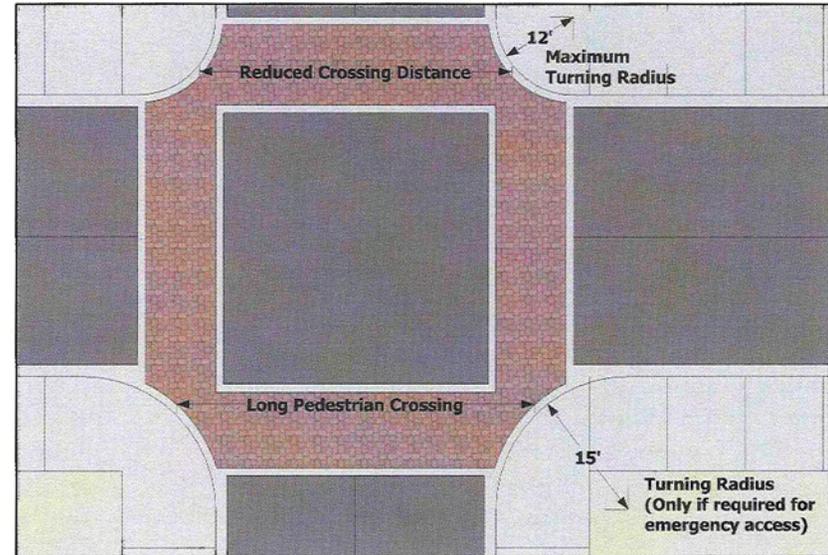
- A Street trees shall be provided along all sidewalks to provide a canopy of shade over the sidewalk. Street trees along sidewalks shall be provided at intervals not to exceed 40 feet. The 40-foot interval may only be modified for driveway curb cuts and to avoid conflicts with public utilities.
- B. Pedestrian-scaled streetlights shall be provided along all sidewalks at intervals not to exceed 80 feet. The pedestrian-scaled streetlight shall not exceed a height of 12 feet. If the streetlight contains an extension arm, the arm shall be directed toward the sidewalk.
- C Tree grates are required for street trees that are planted a paved amenity zone. Tree grates are not required for trees planted within an amenity zone with decomposed granite
- D. A bench, trash receptacle, and recycle receptacle shall be provided within 100 feet of all street intersections along streets with ground-floor commercial uses. These items shall be grouped together and placed between two trees and within 30 feet of a streetlight.
- E. Bollards may be used along the edges of public spaces to help separate vehicular and pedestrian traffic.
- F Four Inverted “U-Shaped” bike rack block shall be provided in the amenity zone of the sidewalk, at intervals not exceed 300 feet.
- G Drinking fountains, kiosks, and other sidewalk amenities may be used throughout the Corridor. If used, these amenities shall be located in the amenity zone of sidewalks.



Layout of sidewalk amenities

Intersections and Crosswalks

- A. To reduce the length of pedestrian crosswalks and the speed of turning vehicles at intersections, the curb radius at street intersections shall not exceed 12 feet unless necessary for emergency vehicle access.
- B. Crosswalks are required at all street intersections and intersections. Mid-block crosswalks are required when the Rail Trail crosses a street. Crosswalks shall be constructed of decorative pavement to clearly mark the pedestrian crossing zone and to help slow the speed of vehicles entering an intersection.
- C. Bulbouts (an extension of the sidewalk into the parking aisle) are required at all crosswalks. Bulbouts shall be constructed with paving materials that are consistent with the adjacent sidewalk.



The curb radius at intersections shall not exceed 12 feet, unless necessary for emergency vehicles



Bulbouts are extensions of sidewalks and are designed similar to the adjacent sidewalk or with decorative paving materials

7.3 *Street Sections*

All existing streets within the Corridor shall be improved and new streets shall be constructed consistent with the Los Banos Improvement Standards and Specifications.

8.0 Public Spaces

Henry Miller Plaza and the Community Center are the only existing public space in the Corridor. This multi-functional plaza is designed around a fountain and statue of Henry Miller. A semi-circular, tree-lined walkway connects H Street to the Rail Trail and surrounds lawn areas.

New public spaces are envisioned within the Corridor. These spaces include Rail Trail Connections, corner plazas, a Median Park, and public spaces associated with the expanded civic center and bus depot.

Figure 8-1 (Public Spaces) shows the required locations of these public spaces. The exact location, shape, and size of the spaces may be modified based adjacent development patterns.

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Figure 8-1: Public Spaces

Public Spaces

A variety of public spaces are required in the Corridor. This figure shows the general locations for each type of public space. The exact location, shape, and dimensions of the public spaces may vary. Standards for each public space and example photographs are provided in Sections 8.1 through 8.6 (see legend in the lower left corner)



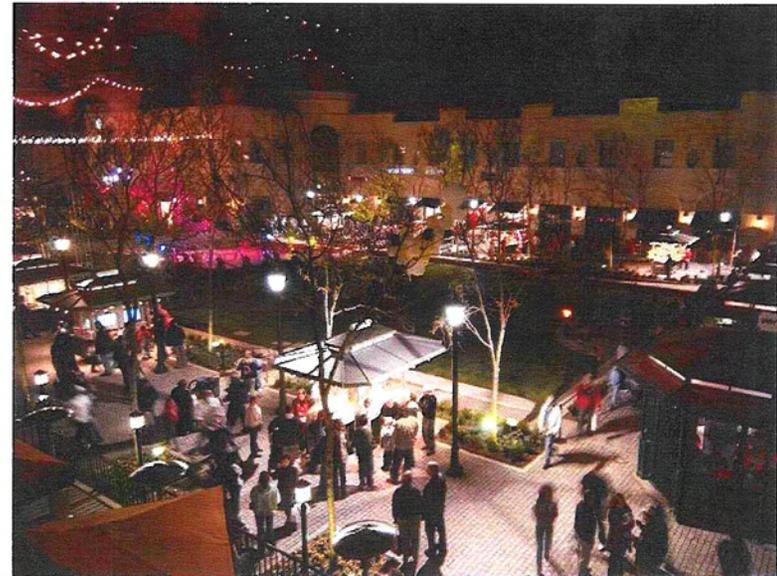
Legend

- Rail Trail Connections (see Section 8.1)
- Corner Plazas (see Section 8.2)
- Median Park (see Section 8.3)
- West Entry (See Section 8.4)
- Civic Center Park (see Section 8.5)
- Bus Depot (see Section 8.6)
- Henry Miller Plaza (Existing)
- Rail Trail (Existing)
- Rail Trail Corridor Boundary

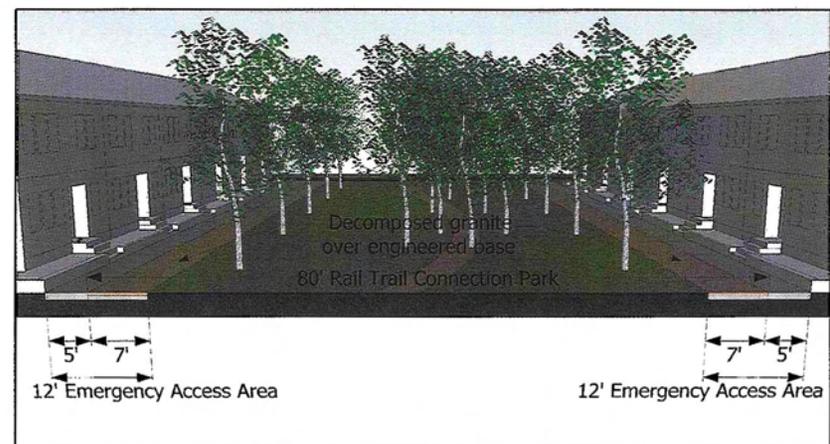
8.1 Rail Trail Connections

The size and shape of Rail Trail Connections shall be established by the standards in Section 4.0 (Subdivision Standards). Rail Trail Connections shall be designed based on the following standards

- A. Rail Trail Connections may be designed as plazas and/or park spaces. In general, these should be designed more like plazas near commercial and live-work buildings and more like parks near buildings with residential uses
- B. Each Rail Trail Connection shall be designed to accommodate emergency vehicle access between H Street, G Street, and the Rail Trail. The emergency access area shall be at least 12 feet wide and located on an engineered base that can hold the weight of a fire engine per Fire Department specifications. Emergency access areas shall be provided along all edges of development sites with In-Line Townhouse Buildings. The required five-foot walkway for In-Line Townhouse Buildings (see Section 5.6) shall be used as part of the emergency access area along with 7 feet of decomposed granite. No permanent vertical obstructions shall be allowed in the emergency access area. Curb-cuts and removable bollards may be provided where emergency access areas intersect with H Street or G Street. Bollard placement and location shall be designed to accommodate bicycle traffic.



Examples of plazas with small product vendors, outdoor seating areas, and performance space for musicians and other performers



C. The following amenities shall be provided within Rail Trail Connections:

- Pedestrian-scaled lighting;
- Shade trees and accent trees;
- Planters (raised planters may serve as benches);
- Benches,
- Trash/recycling receptacles;
- Outdoor dining or seating areas;
- Walkway (which may be a part of the plaza space) that connects the sidewalks on H Street and G Street to the Rail Trail.

D. The following amenities may also be provided within Rail Trail Connections:

- Fountains and other interactive water features,
- Grass areas for informal play;
- Playground and/or tot lot,
- Outdoor stage and/or performance space for musicians, actors, and outdoor events;
- Public art;
- Informational kiosks,
- Educational and/or historical interpretive elements;
- Small product vendors;
- Games (i.e. tables with built-on chess boards); and
- Concierge services.



Example of plazas with outdoor seating areas, pedestrian-scaled lighting, small product vendors, and planters

8.2 Corner Plazas

The size and shape of Corner Plazas shall be established by the standards in Section 4.0 (Subdivision Standards). Corner Plazas shall be designed based on the following standards

- A. Corner plazas shall be designed with decorative pavement interspersed with planters for landscaping.
- B. The following amenities shall be provided within the plazas:
 - Pedestrian-scaled lighting,
 - Planters (may also serve as benches),
 - Benches,
 - Trash/recycling receptacles; and
 - Outdoor dining areas.
- C. The following amenities may also be provided within the plazas:
 - Trees (planted within tree grates),
 - Fountains and other interactive water features;
 - Outdoor stage and/or performance space for musicians, actors, and outdoor events,
 - Public art,
 - Informational kiosks;
 - Educational and/or historical interpretive elements;
 - Small product vendors,
 - Games (i.e. tables with built-on chess boards); and
 - Concierge services.



Envisioned corner plazas at the intersection of H Street and Seventh Street

8.3 Median Park

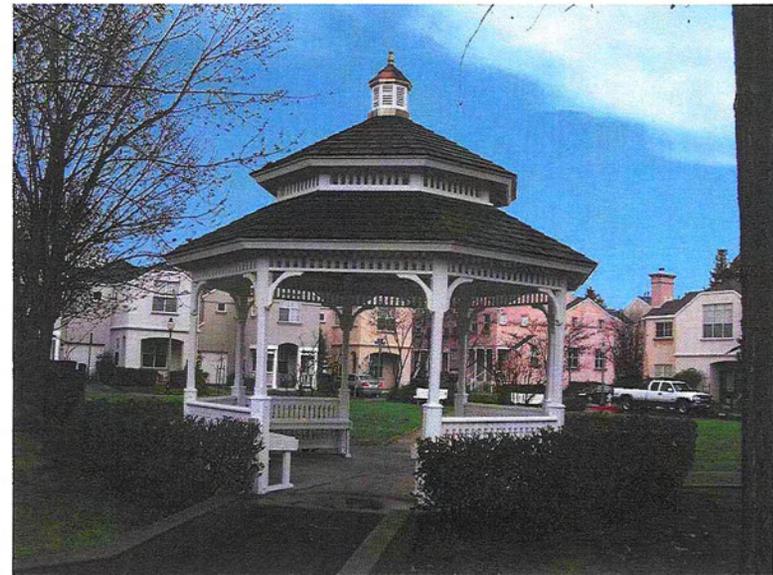
The size and shape of Median Park shall be established by the standards in Section 4.0 (Subdivision Standards). The Median Park shall be designed based on the following standards.

A. The Median Park shall primarily consist of green space for informal gathering and recreation. The following features shall be included in the park:

- Pedestrian paths
- Pedestrian-scaled lighting;
- Grass areas for informal play,
- Playground and/or tot lot;
- Gazebo or other centrally located gathering space,
- Trees;
- Benches, and
- Trash/recycling receptacles

B. The following amenities may also be provided within the Median Park:

- Fountains and other interactive water features,
- Public art;
- Informational kiosks;
- Educational and/or historical interpretive elements;
- Games (i.e. tables with built-on chess boards, enlarged board games, tic-tac-toe, hopscotch, etc); and
- Picnic areas;
- Community gardens.



8.4 West Entry

The existing segment of the Rail Trail adjacent to Second Street and H Street shall be redesigned to create an entry plaza or park. The plaza or park will be designed based on the following standards:

- A. The asphalt surface of the trail shall be replaced with decorative concrete pavers.
- B. A seating wall shall be constructed around the existing triangle shaped planter area at the Rail Trail intersection
- C. The following amenities may also be provided within the West Entry and adjacent to the existing Rail Trail:
 - Benches, trash/recycling receptacles;
 - Fountains and other interactive water features;
 - Public art;
 - Informational kiosks,
 - Educational and/or historical interpretive elements, and
 - Games (i e. tables with built-on chess boards, enlarged board games, tic-tac-toe, hopscotch, etc.).



Envisioned West Entry

8.5 Civic Center Park

The Civic Center is envisioned to include a variety of public facilities such as a community center, public market, senior housing complex, museum, and library. The civic center area may also be expanded to include an aquatic center just north of the Corridor. Public spaces would surround the buildings within the Civic Center. The public spaces shall be designed based on the following standards:

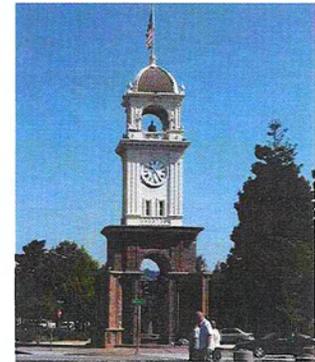
A. The following amenities shall be provided within the park:

- Pedestrian-scaled lighting;
- Grass areas for informal play,
- Trees;
- Benches; trash/recycling receptacles;
- Picnic areas; and
- A central tower element located north of the Rail Trail along the centerline of Sixth Street. The purpose of the tower element is to visually connect the Civic Center to Downtown Los Banos and Henry Miller Plaza.

B. The following amenities may be provided within the park:

- Fountains and other interactive water features;
- Outdoor stage and/or performance space for musicians, actors, and outdoor events;
- Public art,
- Informational kiosks;
- Educational and/or historical interpretive elements,
- Games (i.e. tables with built-on chess boards, enlarged board games, tic-tac-toe, hopscotch, etc.);

- Demonstration gardens with native and drought-tolerant landscaping; and
- Playground and/or tot lot.

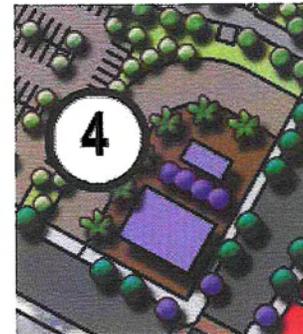


Examples of features envisioned within the Public Spaces within the Civic Center

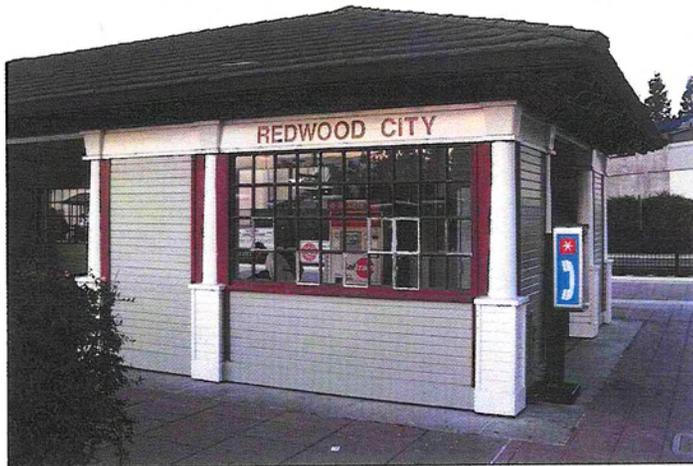
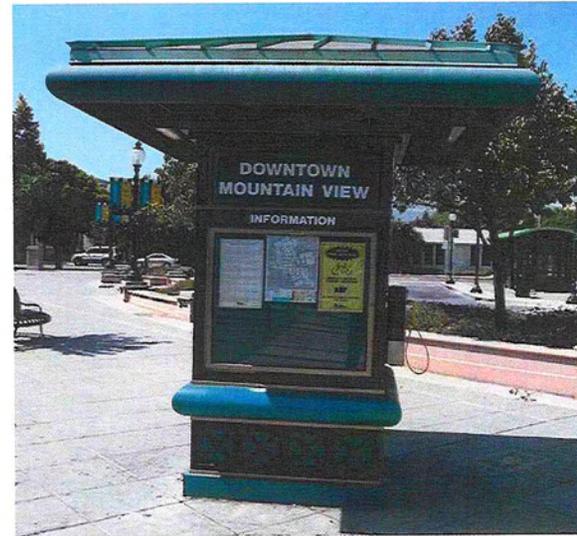
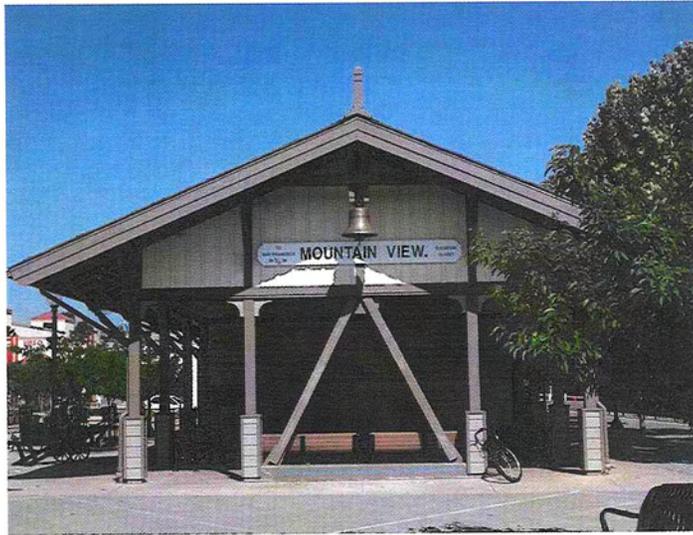
8.6 Bus Depot

A bus depot is envisioned at the corner of H Street and Fourth Street. The bus depot would serve local, regional, and national bus lines. The facilities would include a bus loop with bays for bus stops, a building with ticketing facilities and retail and restaurant/cafe uses, covered waiting areas, and a parking lot. The following standards shall apply to the bus depot:

- A. A bus loop with bays for bus stops shall be located around a central plaza. The turnaround area shall be generally “L” shaped and be designed for one way bus travel. One driveway access for the bus loop shall be provided on H Street and one on Fourth Street.
- B. A central plaza shall be provided in the center of the bus loop. This plaza shall open up to the sidewalk and provide space for the buildings and waiting areas below
- C. A central building shall be provided to accommodate ticketing facilities, waiting areas, retail/restaurant uses, and office space for transit operators.
- D. A separate building or covered area shall also be provided for additional waiting areas, ticket vending machines, and informational kiosks.
- E. A parking lot shall be provided to accommodate employees, transit riders, and customers. A walkway shall connect the parking lot to the central plaza.
- F. The following amenities shall be provided within the central plaza:
 - Pedestrian-scaled lighting;
 - Trees (planted within tree grates),
 - Planters (may also serve as benches);
 - Benches,
 - Trash/recycling receptacles; and
 - Informational kiosks with maps, schedules, next bus arrival times, and other relevant information.
- G. The following amenities may also be provided within the central plaza:
 - Bus shelters or other shade/protective structures,
 - Outdoor dining areas;
 - Educational and/or historical interpretive elements;
 - Small product vendors, and
 - Games (i.e. tables with built-on chess boards).



Envisioned location and layout of the bus depot at the intersection of H Street and Fourth Street



Examples of covered waiting areas with ticket vending machines

Example of an informational kiosk with transit information (top) and a bus depot with a pedestrian path connecting parking to bus stops (bottom)

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9.0 Administration

9.1 Applicability

This Regulating Code applies to all future development and redevelopment projects within the Rail Trail Corridor (see Figure 1-2. Rail Trail Corridor Boundary) As intended, this Code regulates massing of the proposed development through height restrictions, setbacks and build-to-lines, building form and orientation, as well as mix of land uses.

9.2 Severability

If any part or portion of this Regulating Code is rendered or declared unconstitutional or invalid by a court of competent jurisdiction, such invalidation of such part or portion of this Regulating Code shall not invalidate the remaining portions thereof, and they shall remain in full force and effect

9.3 Development Review Process

Project applicants utilizing this Regulating Code for their project in the Corridor shall adhere to the necessary steps for the development application and review process.

Pre-Submittal Conference

The project applicant (i.e. developer, property owner, or other authorized representative) shall meet with the Los Banos Community Development Department (City Staff) to discuss proposed plans and review submittal requirements. The project applicant may also meet with staff from other departments to address critical issues

Application Submittal

After the pre-submittal conference, the project applicant will submit a completed project application to the City of Los Banos Community Development Department

Project Review for Completeness

Within 10 business days of submitting an application, City Staff will provide a notification of completeness to the project applicant. This notification will state that the application is complete or provide requirements for submitting any additional information that may be required. A project application is not considered complete until all the application materials are submitted in the correct form and numbers.

Public Hearing Review and Approval Process

All projects shall require approval by the Planning Commission. The review and approval process for these types of projects shall be defined by the Zoning Ordinance (Application Processing, Site Plan Review Procedure, Administrative Permits, Use Permits, Variances and Appeals).

Appeals

All appeals will be processed in compliance with the Zoning Ordinance.

Variance

A Variance may be permitted when practical difficulties, unnecessary hardship, or results inconsistent with the general vision of this Regulating Code result through the strict and literal interpretation and enforcement of a standard. The sole purpose of a variance is to grant relief from a specific regulation because of a special circumstance related to the property that would deprive the property from privileges commonly enjoyed by other properties in the same vicinity and

zone. A variance shall not be granted if it gives special privileges not shared by other property in the same vicinity and zone. The process for obtaining a variance from this Code shall be regulated by the Zoning Ordinance.

The following findings shall be made to issue a variance:

- Because of special circumstances applicable to the property, including size, shape, topography, location or surroundings, the strict application of this Regulating Code deprives such property of privileges enjoyed by other properties in the vicinity that are subject to the identical development standards.
- The adjustment thereby authorized does not constitute a grant of special privileges inconsistent with the limitations upon other properties in the vicinity that are subject to the identical development standards.
- The adjustment does not authorize a use or activity that is not otherwise expressly authorized by land use regulations governing the parcel of property.

The Planning Department shall keep a permanent record of all decisions of the Planning Commission, as well as the ultimate disposition of those applications that are appealed. The record shall be available for public review

9.4 Amendments to the Regulating Code

The City Council may amend this Regulating Code, including text and figures, whenever the public necessity, convenience, or general welfare requires such amendment. The boundaries of the Corridor may also be expanded to include adjacent properties that are currently not within the Corridor. Property owners may also request amendments to the Regulating Code. Fees and Costs

Each development application in the Corridor will be subject to certain fees. The fees will reimburse the City for the

reasonable costs of administering this Regulating Code and other applicable City ordinances. Such fees shall be set by resolution of the City Council. Project applicants shall pay all required fees when the application is submitted to the Community Development Department.

9.5 Applicable Sections from the Zoning Ordinance

In addition to the provisions of this Regulating Code, all development applications and activities within the Corridor are also subject to the City of Los Banos Zoning Ordinance

Should any conflict arise between the provisions of this Regulating Code and the City of Los Banos Zoning Ordinance (Title 9 of the City of Los Banos Municipal Code), the provisions of the Municipal Code shall apply

9.6 Applicable Sections from the Zoning Ordinance

In addition to the provisions of this Regulating Code, all development applications and activities within the Corridor are also subject to the City of Los Banos Zoning Ordinance. Should any conflict arise between the provisions of this Regulating Code and the City of Los Banos Zoning Ordinance (Title 9 of the City of Los Banos Municipal Code), the provisions of the Municipal Code shall apply

10.0 Definitions

The following definitions are provided for terms used within this Regulating Code. If a word or phrase is not defined, then the word or phrase shall be defined as set forth in any recognized treatise or authority on zoning, planning, or architecture. A dictionary shall be used to define common words or phrases.

A

Alleys: A privately owned and maintained roadway that provides access from streets to the rear of each development sites within a block

Allowed Use: A use that is permitted to occur in a specific building at a designated location

Apartment: A residential unit that is within a larger complex of residential units, all of which are rented by separate individuals or households

Awning: A covered architectural projection that extends from the exterior wall of a building for the purpose of providing shade or shelter

B

Balcony: A platform that projects from an upper floor of a building.

Bay Window: A window and related structure that extends outward from an exterior building wall and thereby forms an alcove in the adjoining interior space.

Block: An area that is completely surrounded by a combination of streets, the existing Rail Trail, a planned Rail Trail Connection, or the planned Fifth Street Public Parking Lot

Build-to-Line (BTL): A line parallel to a property line along which a front facade must be built along. BTL standards are established by building type in Section 5.0.

Building Facade: A wall or series of walls that together make up a side of a building.

Building Frontage: The side of the building that faces the front of the parcel.

Building Height: The height of the building as measured by the number of allowed floors. Within the Rail Trail Corridor, the maximum building height is limited by the maximum number of floors and the maximum ceiling height for each floor

Bulbout: An extension of the sidewalk into the parking lane at intersection and mid-block crosswalks. Bulb-outs reduce the length of pedestrian crossings and help to slow vehicle traffic.

Bulkhead: A short wall or protective barrier located below storefront windows. Bulkheads sometimes contain landscaping

C

City: City of Los Banos.

Civic Center: The area bounded by the Second Street, G Street, the existing alley, Seventh Street, and the Rail Trail. This area may accommodate a community center, public market, senior housing, library, and/or museum. An expansion of this area to F Street could accommodate an aquatic center

Clear Zone: A zone at alley intersections that has development restrictions to maintain visibility from one alley to another

Cluster Townhouses: Two story residential units that are placed side-by-side to form two rows of houses that face each other and are separated by a common green. The garage is accessed from a rear alley

Commercial Buildings: One or two story buildings designed for a mix of commercial uses, including retail, restaurants, cafes, and office uses.

Commercial Use: Retail, service, and office uses.

Conditionally Permitted Use: A use that requires a conditional use permit (CUP) to be permitted within a specific building type.

Condominium: A residential unit that is within a larger complex of residential units, each of which is owned by separate individuals or households. Common areas, such as hallways, grounds, recreational facilities, and parking facilities, are owned in common and maintained by a homeowner's association.

Corner Plaza: One of two planned plazas located at the northwest and northeast corners of Seventh Street and H Street

Cornice: A horizontal molded projection that crowns or completes a building facade. It is the uppermost section of moldings along the top of the wall or just below a roof.

Corridor, The: The Los Banos Rail Trail Corridor (the project area for this Regulating Code).

Cottage Houses: One and two story houses that have yards and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.

Courtyard: A common area that is bounded on three or four sides by buildings

Courtyard Flats: Two story buildings designed for stacked residential units (condominiums or apartments). The residential units create a "U" shaped building that surrounds a common courtyard

D

Development Site: A portion of a block where an individual building type and related site features are allowed.

E

No definitions.

F

Facade: See "Building Facade"

Facade Length: The distance from one side of the building's frontage to the other side of the building frontage.

Family Entertainment (Use): Any business that generates income by selling admission or use fees for indoor or outdoor entertainment uses. Family entertainment uses may also generate secondary income from dining facilities (cafes, restaurants, and catering services), on-site services, and equipment sales/rental that is related to the family entertainment use. Family entertainment uses include (but are not limited to) bowling alleys, party room facilities, miniature golf courses, arcades, laser tag arenas, rock climbing facilities, bumper car or boat facilities, batting cages, and skating rinks.

Fifth Street Public Parking Lot: A planned public parking lot located within the former right-of-way of Fifth Street between H Street and the existing Rail Trail.

Floor / Floors: Enclosed building space (building levels) that is bounded by walls, floors, and ceilings.

Front Porch: An area connected to the ground floor of a building that is covered by a roof, but does not contain glass windows, walls, or fences (except railings and support posts). Front porch standards are established in Section 6 14 (Front Porches and Stoops).

Full Mansard Roof: A roof designed with two slopes on each side of the roof and the lower roof slope is steeper than the upper roof slope. The upper roof slope is sometimes flat.

G

Gabled Roof: A roof designed with sloping planes (usually of the same pitch) on two sides of the roof ridge and a gable (a triangle-shaped building form that is formed by joining two sloping roof planes) at each end of the ridge.

Garage: See "Parking Garage"

Ground Floor: The first (closest to the finished site grade) habitable building floor of a building.

H

Henry Miller Plaza: An existing semi-circular public plaza that is located along H Street at the terminus of Sixth Street.

Hip Roof: A roof designed with sloping planes on two sides of a ridge. Hip roofs are not designed with gables.

Home Occupation: An operation conducted in a residential unit by a resident of that unit as a secondary use. Advertising signs, displays, stock or commodities sold on the premises, employees, and large or noisy mechanical equipment are prohibited.

I

In-Line Townhouses: Two story residential units that are placed side-by-side to form a row of houses that face a street

or Rail Trail Connection. The garage is accessed from a rear alley.

J

Joint Use Agreement: An agreement between two parties regarding the shared use of facilities (i.e. a parking area that serves multiple buildings or uses may require a joint use agreement to satisfy the parking requirement for all buildings or uses).

K

Kickplate: A plate (often made of metal) fastened to the lower portion of a door to prevent damage to protect the door's surface from shoe marks.

L

Live-Work Buildings: One to two story buildings that contain units in which the owner can both live and operate certain types of office and service businesses. The space designed for office and service businesses may need to be designed to commercial building code requirements depending on the types of businesses that would be allowed by the development.

Lodging (Use): Bed and Breakfasts, Hotels, Motels, Suites and similar uses.

Lot: A subdivision of a development site that is also known as a parcel. Lots and parcels are legal boundaries of land on which development may occur. Generally buildings are not permitted to be constructed across lot or parcel lines.

M

Marquee: A permanent roof-like structure that projects from the building wall to provide shade and shelter. Marquees sometimes have signs.

Mixed-Use Buildings: Two to three story buildings designed for a mix of commercial and residential uses. The ground floor generally contains storefronts for retail stores, restaurants, and cafes. Upper floors generally contain apartments, condominiums, and/or office space.

Median Park: A neighborhood park located within the median of a planned street that would extend from the San Juan Avenue Extension to the Santa Rita Avenue Extension.

N, O

Office (Use): Any business establishment that provides administrative or clerical work or service-related work that does not require the customer or client to be on site to receive the service. Office uses include (but are not limited to) banks, financial institutions, and administrative and professional offices for architects, engineers, consultants, marketing agents, travel agents, insurance and real estate agents, and lawyers.

P

Parapet: A low wall projecting from the edge of a roof. Parapets may be designed with details and cornices.

Parking and Service Zone: The area of a development site that may be occupied by parking, trash and recycle receptacles, and loading and service zones.

Parking Garage: An enclosed building that accommodates parking spaces for residential units. Parking garages may be attached or detached from the building and may be covered by habitable space.

Paseo: A pedestrian-only corridor that is lined on either side with buildings that generally have shops, restaurants and cafes, or entertainment uses on the ground floor.

Paved Zone: The area of a sidewalk adjacent to a building. This area shall be paved and is generally free of items that may impede pedestrian circulation, except as specified in Section 7.2 (Sidewalk Standards).

Plaza: "Public Plaza" that may not be open to the public.

Podium Parking: Parking spaces that are covered by the ground floor of a building and are completely enclosed by walls. Podium parking may occur at or below the grade of the adjacent sidewalk.

Porch: See "Front Porch"

Promenade, The: See "Rail Trail Promenade"

Public/Civic Buildings: Buildings designed for public or civic purposes, such as a community center, bus depot, library, museum, public market, or aquatic center.

Public / Private Partnership: A development agreement entered into by the City of Los Banos and the property owner or developer, in which both parties participate in the planning, financing, and construction of a development project.

Public Plaza: A type of public space that includes hardscape (paving) and is open to the public. Plazas often have buildings along at least one edge and are usually accessed from sidewalks.

Public Space: An outdoor gathering area that is available for use to the public. Types of public spaces envisioned for the Corridor include Rail Trail Connections, Corner Plazas, a Median Park, Civic Center Public Spaces, and a Bus Depot. Standards for each of these public spaces are established in Section 8.0 (Public Spaces).

Q

No definitions.

R

Rail Trail: An existing multi-purpose trail that is located on the abandoned Union Pacific Railroad corridor and extends from Second Street to the intersection of Mercy Springs Road and Pacheco Boulevard

Rail Trail Connection: A planned park or plaza space that connects to an existing or future street to the Rail Trail.

Rear Yard Townhouses: Two story residential units that are placed side-by-side to form a row of houses that face a street. Each unit contains a yard that is located between the unit and an attached or detached garage that is accessed from a rear alley. A second unit is allowed above the garage.

Regulating Code: A zoning code that emphasizes the form and location of buildings over the use of buildings, unlike conventional "Euclidean" zoning codes, which emphasize land use over other site and building design.

Regulating Plan: A plan or map that designates how site design and building form standards are applied to specific development sites.

Residential (Use): A use that accommodates living space, including apartment units, condominium units, and single-family homes

Retail (Use): Any business that generates income by selling a tangible good or product. Retail uses include (but are not limited to) bakeries, bars, cafes, coffee shops, restaurants, book stores, clothing/apparel stores, sporting good stores, flower shops, electronic and appliance stores, grocery stores, pet stores, jewelry stores, shoe stores, gift shops, home furnishing stores, toy stores, specialty retail stores, card shops, art supply stores, and toy stores.

S

Second Unit: A separate dwelling unit located above the garage of cottage house or rear yard townhouse. Second units have separate and permanent provisions for living, sleeping, cooking, and sanitation. Second units shall range from 400 to 800 square feet, shall have a separate building entrance, and shall be designed with compatible materials, colors, and architectural styles and details as the primary unit on the property.

Senior Housing: Residential units (often apartments or condominium) that are designed for senior citizens. These units are sometimes smaller than equivalent units and parking requirements may be reduced since the demand may be lower.

Service (Use): Any business in which income is generated primarily from customers that receive a service performed on-site. Service businesses may also generate secondary income from retail sales that are related to the service. Service businesses include (but are not limited to) hair salons, barber shops, health clubs and gyms, movie theaters, pet grooming, photography studios, massage parlors, tutoring, dance and art instructions, martial arts studios, laundry and dry cleaning services, repair shops (appliances, televisions, radios, and computers), tattoo parlors/body piercing, veterinary clinics, counselors, therapist, medical services (doctors, chiropractors, dentists and orthodontists offices, medical laboratories, etc), and blue printing/copy centers.

Setback: An area in which buildings or other structures shall not occur

Sidewalk Amenities: Items placed along the sidewalk for the use of pedestrians and to create a safer, more attractive streetscape. Standards for sidewalk amenities are established in Sections 7.1 (General Streetscape Standards) and 7.2 (Sidewalk Standards).

Stacked Flats: Two to three story buildings designed with stacked residential units (condominiums or apartments). Parking is provided in a podium parking level or a surface parking lot that is surrounded by the residential units.

Stoop: A platform in front of a building entrance, which may or may not be covered by a roof. Standards for stoops are established in Section 6.14 (Front Porches and Stoops).

Surface Parking: Parking spaces that are not covered by a building and are not enclosed by walls. Surface parking is also known as a "parking lot".

T

Townhouse: A building that is designed for a single-family unit and is attached to at least one adjacent unit, which may have common walls. See "Cluster Townhouse", "In-Line Townhouse", and "Rear Yard Townhouse" for specific types of townhouses allowed in the Corridor

Transom Windows: A short window placed above a door or window. Transom windows are generally no more than 2 feet in height and are usually as wide as the door or window that they are on top of

Transparency: A measurement of how transparent or "see through" a window, fence, or wall is. Transparency is the opposite of opacity (i.e. a fence that is 25% transparent is 75 opaque).

Trellis: A structure, usually made from interwoven wood or metal pieces, that is attached to the roof or building wall used for shade or to support climbing plants.

Tuck-Under Parking: Parking spaces that are covered by the upper floor of a building, but are otherwise open

U

Upper Floor: A floor that is above the ground floor

V

No definitions.

W

Walkway: A pedestrian path that is similar to a sidewalk, but may not be located adjacent to a street. Walkways are often provided on private development and within public spaces

West Entry: A planned plaza at the western entrance to the Rail Trail on Second Street.

X, Y

No definitions.

Z

Zoning Ordinance: Title 9 (Planning and Zoning) of the City of Los Banos Municipal Code, and all subsequent chapters, articles, and sections.